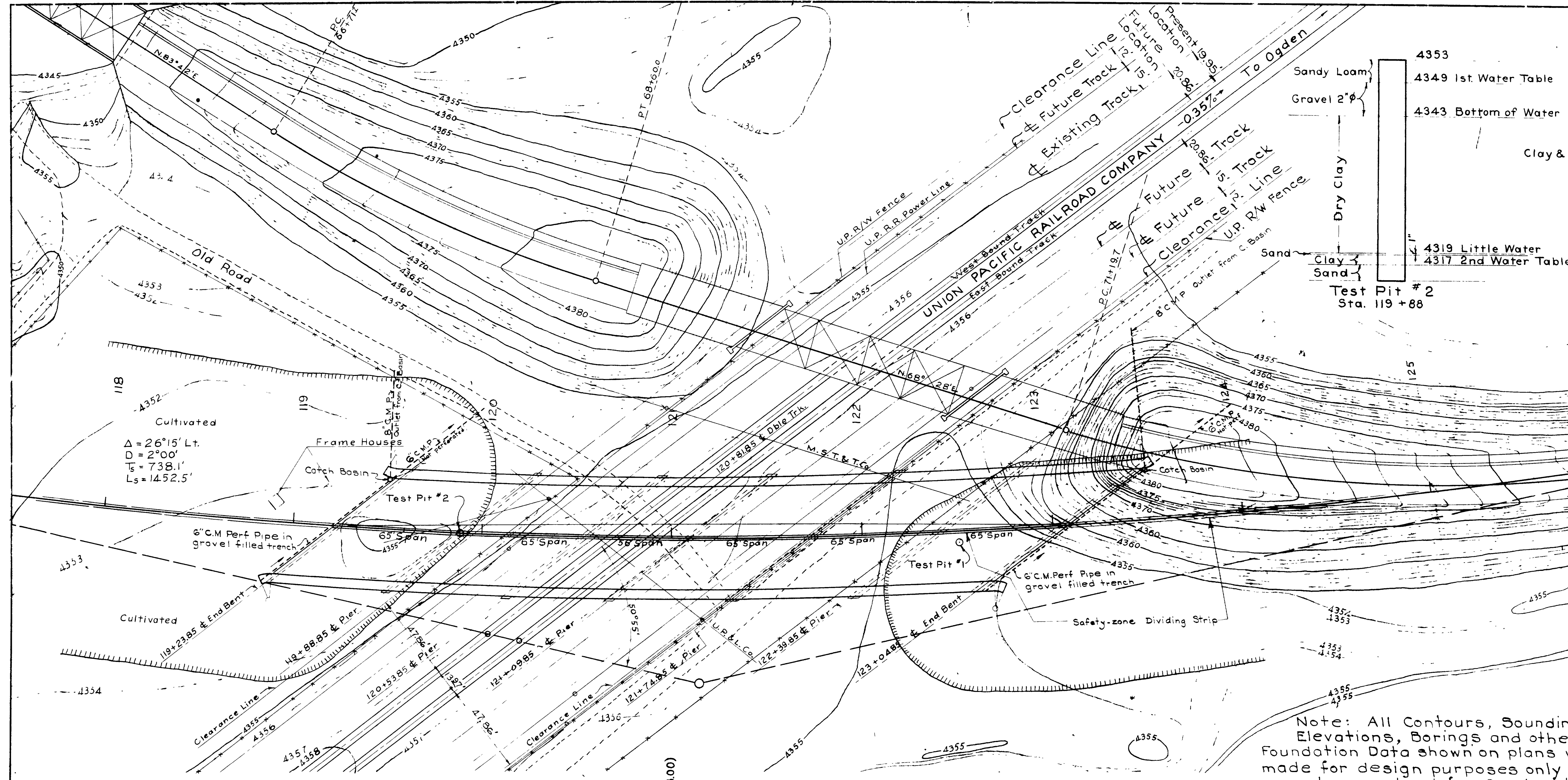
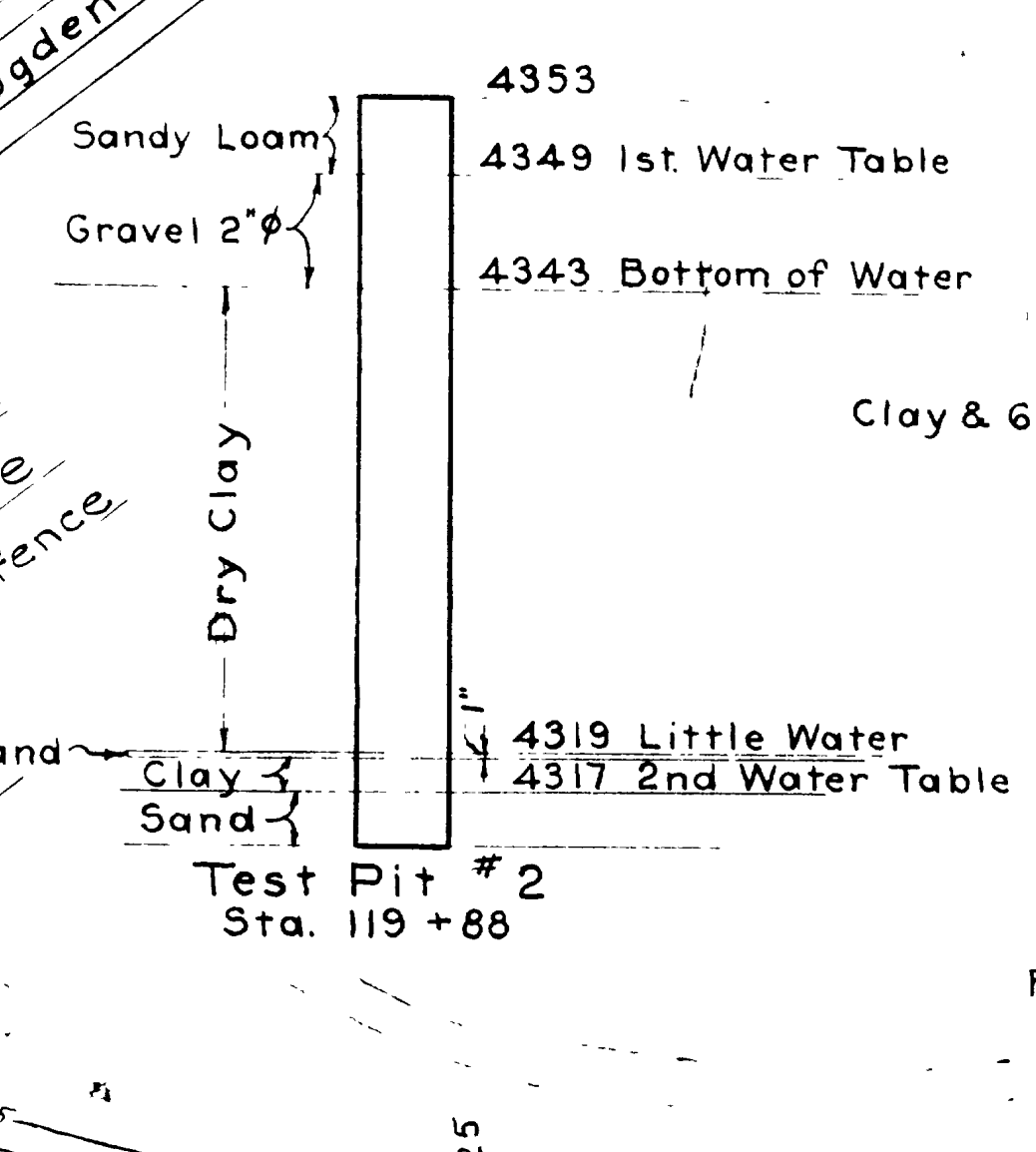


F.A.P.					
ED. ROAD DIST. NO.	STATE	U.S. PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
12	UTAH	31			



Note: All Contours, Sounding Elevations, Borings, and other Foundation Data shown on plans were made for design purposes only and are not guaranteed for construction.



Test Pit #1  
5' Rt. Sta. 122+50

### GENERAL NOTES

Materials, construction and workmanship shall be in accordance with the State Standard Specifications for Road and Bridge Construction, edition of 1939 and supplements thereto.  
 All concrete shall be Class "A" and all exposed edges shall be chamfered 1" unless noted otherwise.  
 Aggregate for Class "A" concrete shall be 1" thru-out.

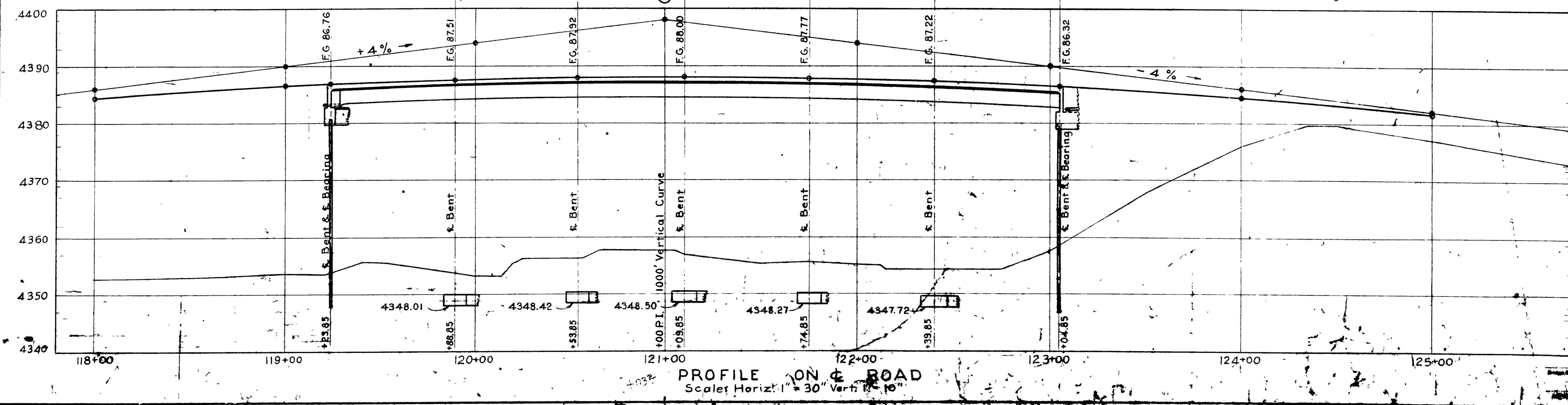
### DESIGN DATA

A.A.S.H.O. Specifications  
 H15-S12 Loading,  $f_c = 1000 \text{ #/a}^2$ ,  
 $f_s = 18,000 \text{ #/a}^2$

### QUANTITIES

- Excav. for Structures 2060
- Concrete, Class "A" 2520
- Reinforcing Steel 669,200
- Struct. Steel (& Misc. Metals) 106,000
- Concrete Hand Rail 78
- 6" φ C.M. Pipe (Perforated) 10
- 6" φ C.M. Pipe (Not perforated) 10
- 8" φ C.M. Pipe " 10
- Pre-cast Curbs (Dividing Strip) 32
- 16" φ #11-Ga. Steel Shell  
Cast in place Conc. Piles (44 @ 33'-0") 145

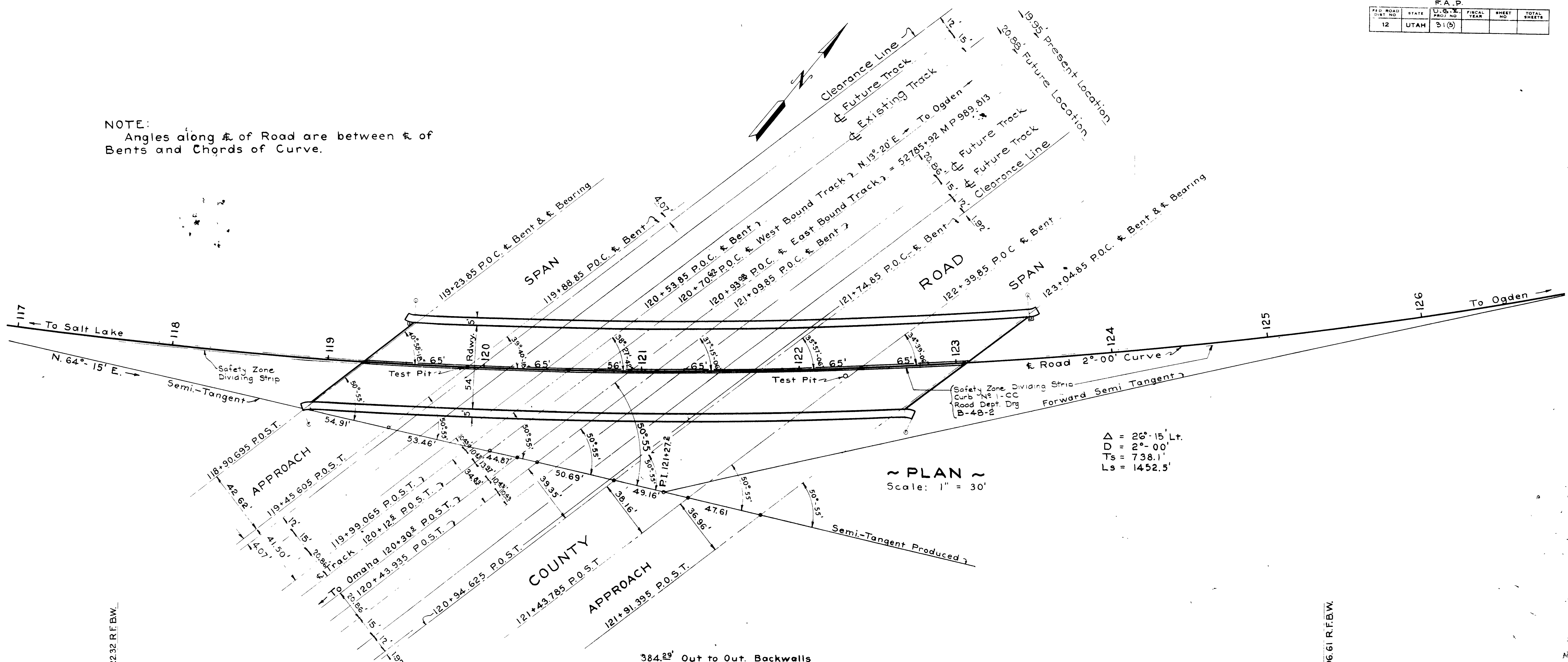
### SITUATION PL



UTAH STATE ROAD C  
 RIVER BRIDGE  
 OVER RIVER  
 S.D. RIVER

F.A.P.					
F.I.D. ROAD DIST. NO.	STATE	U.G.I. PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
12	UTAH	31(6)			

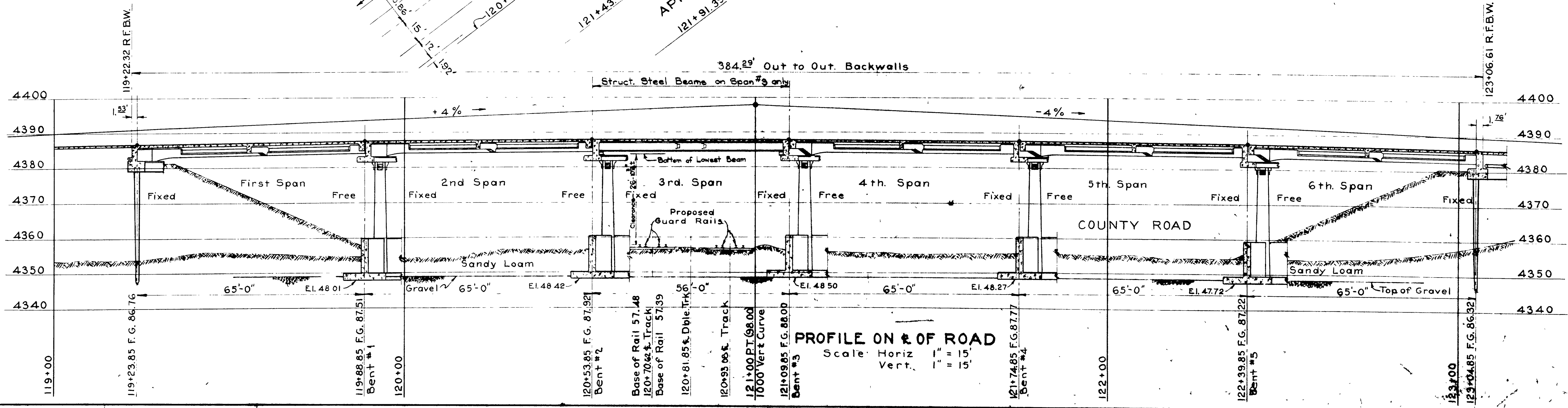
NOTE:  
Angles along  $\&$  of Road are between  $\&$  of Bents and Chords of Curve.



~ PLAN ~  
Scale: 1" = 30'

$\Delta = 26^{\circ} 15' Lt.$   
 $D = 2^{\circ} 00'$   
 $Ts = 738.1'$   
 $Ls = 1452.5'$

NO.	DATE	BY
1	5/10/45	RAW
2	5/19/45	RAW



PROFILE ON  $\&$  OF ROAD  
Scale: Horiz 1" = 15'  
Vert. 1" = 15'

MAR 1 1946

*Edmund*  
CIVIL ENGINEER  
*Paul Martin*  
APPROVED

LAYOUT

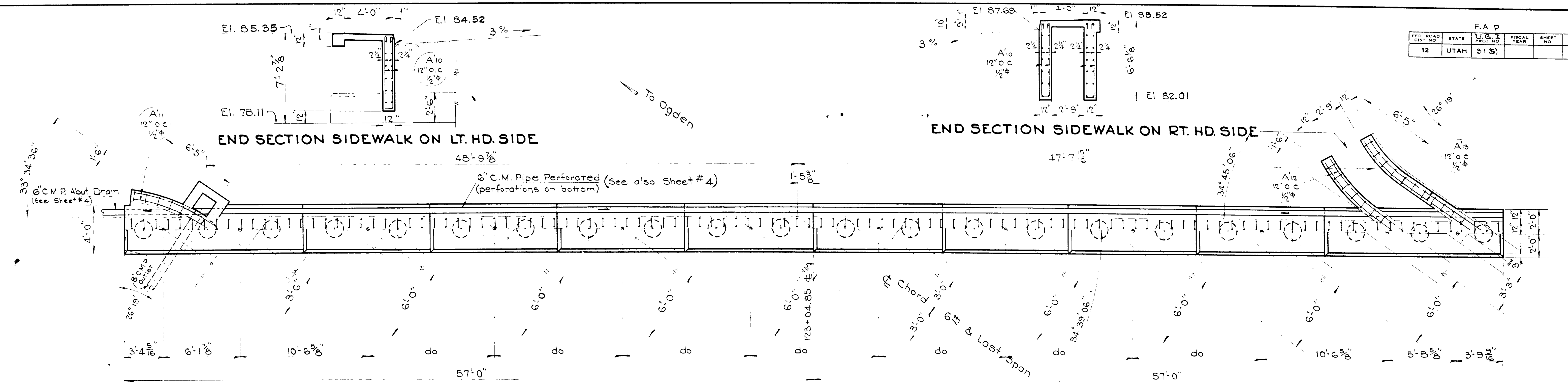
Sheet 2 of 25 sheets  
UTAH STATE ROAD COMMISSION  
SALT LAKE CITY - UTAH  
BRIDGE DEPARTMENT  
**RIVERDALE OVERHEAD  
OVER U.P.R. TRACKS**  
Sta. 121+14.47 to U.G. I-310  
Riverdale Revision 1 Weber Co.  
DESIGNED BY: F.M.F. SCALE: As Shown  
CHECKED BY: F.M.F.  
APPROVED BY: [Signature]  
DATE: 3-22-45



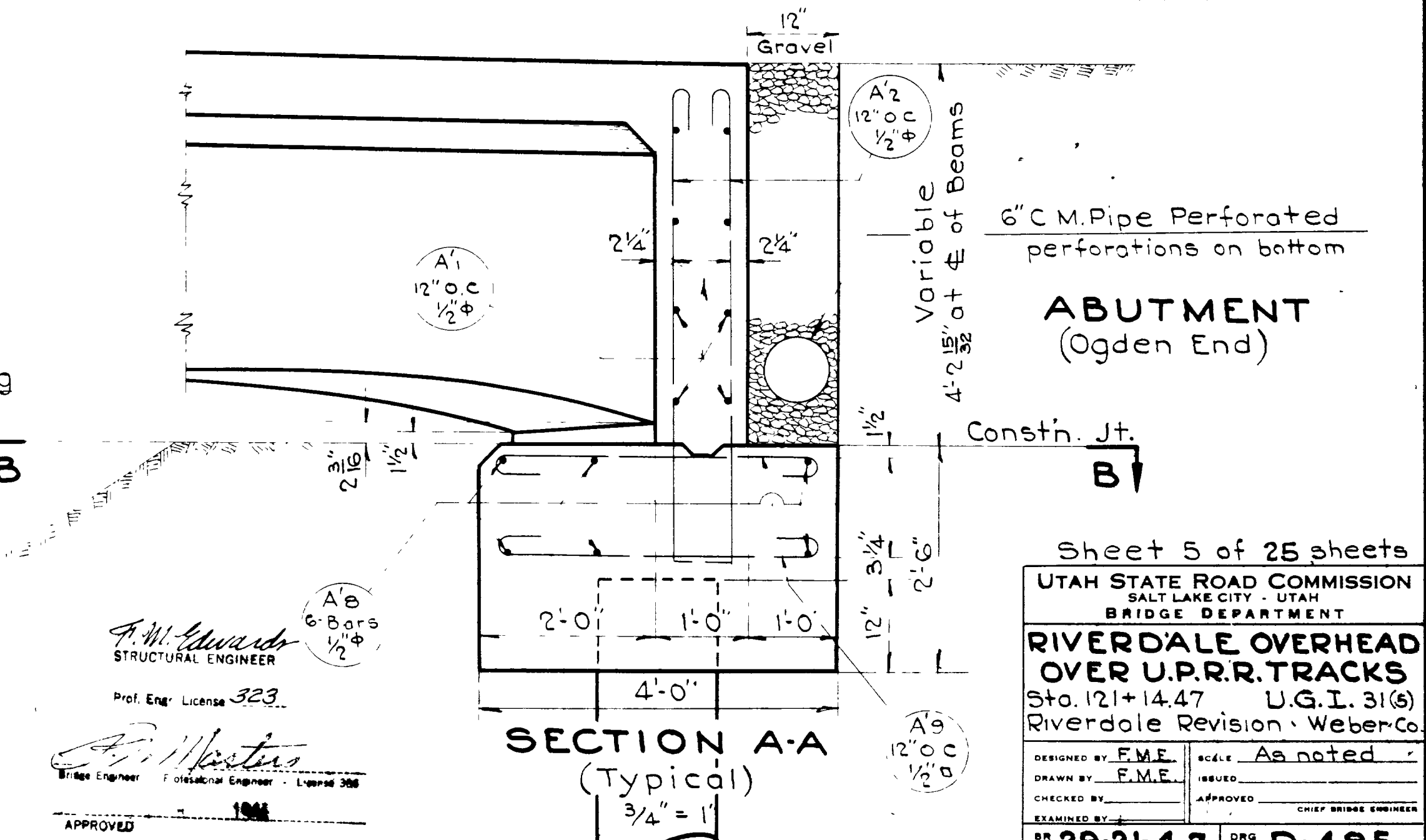
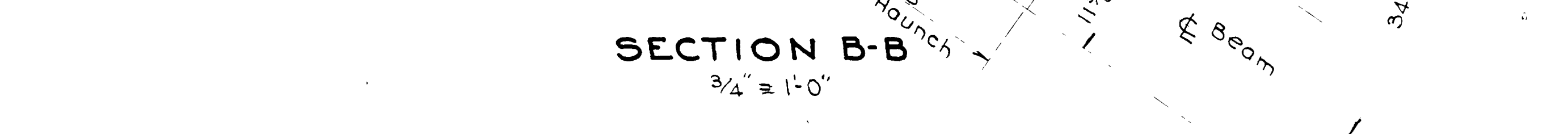
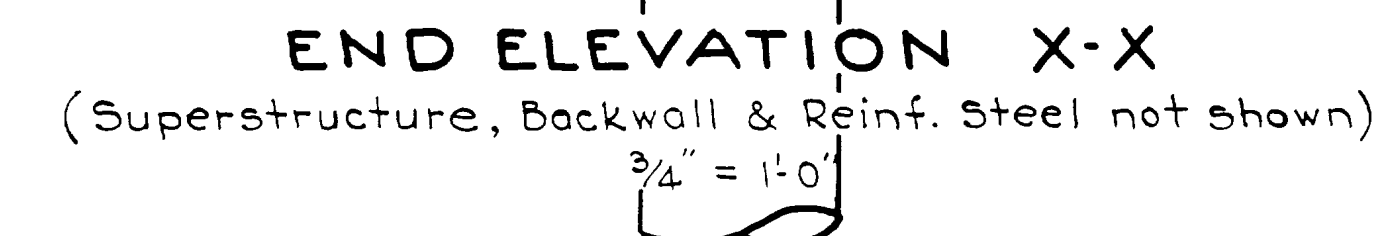
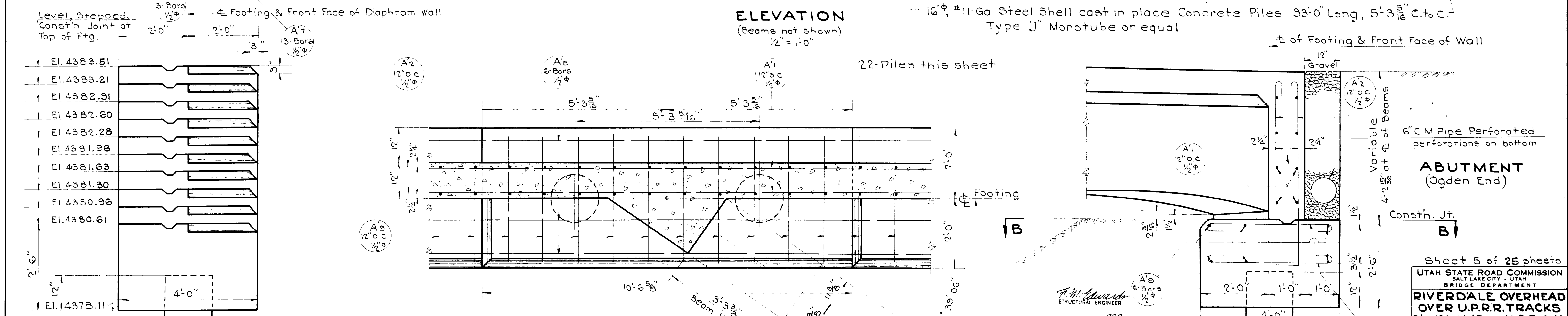
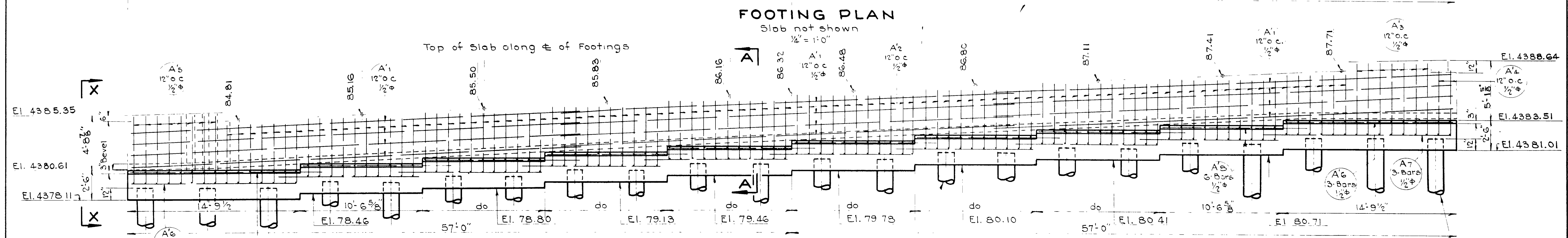




F.A.P.					
FED. ROAD DIST. NO.	STATE	U.G.I. PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
12	UTAH	31(5)			



REVISIONS	DATE	BY

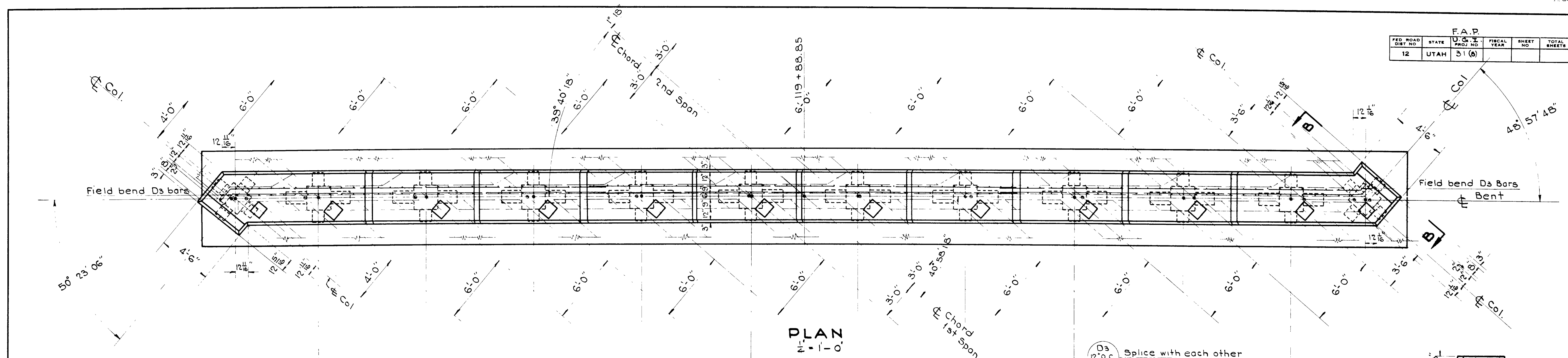


*F. M. ...*  
 STRUCTURAL ENGINEER  
 Prof. Eng. License 323  
 APPROVED

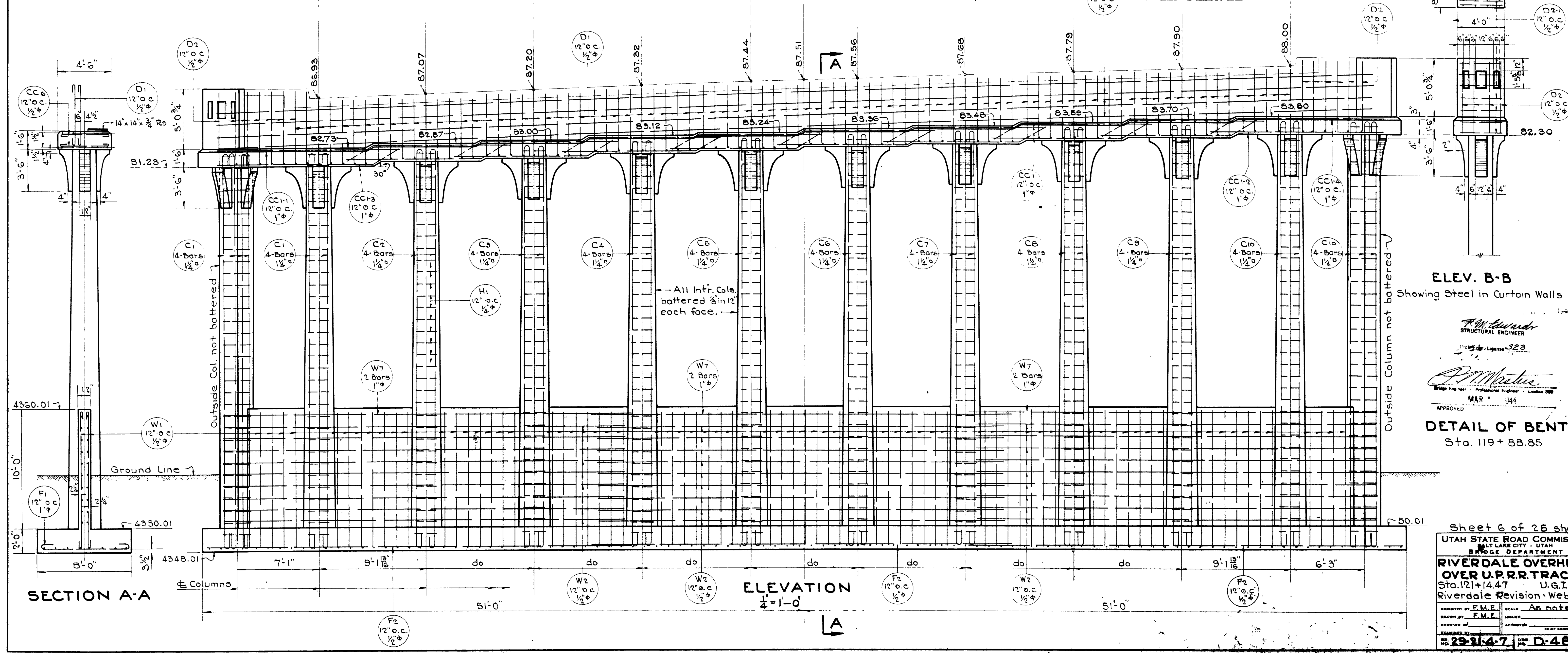
Sheet 5 of 25 sheets  
 UTAH STATE ROAD COMMISSION  
 SALT LAKE CITY - UTAH  
 BRIDGE DEPARTMENT  
**RIVERDALE OVERHEAD  
 OVER U.P.R.R. TRACKS**  
 Sta. 121+14.47 U.G.I. 31(5)  
 Riverdale Revision - Weber Co.  
 DESIGNED BY: E.M.E. CHECKED BY: As noted  
 DRAWN BY: E.M.E. ISSUED  
 CHECKED BY: APPROVED  
 EXAMINED BY: CHIEF BRIDGE ENGINEER  
 BR NO 29-21-4-7 DRG NO D-485



FED. ROAD DIST. NO.	STATE	U.G.I. PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
12	UTAH	31 (6)			



PLAN  
1/4" = 1'-0"



ELEVATION  
1/4" = 1'-0"

ELEV. B-B  
Showing Steel in Curtain Walls

*F.M. Edwards*  
STRUCTURAL ENGINEER  
License 323  
*P. M. Weber*  
Professional Engineer License 289  
MAR 7 1944

DETAIL OF BENT  
Sta. 119 + 88.85

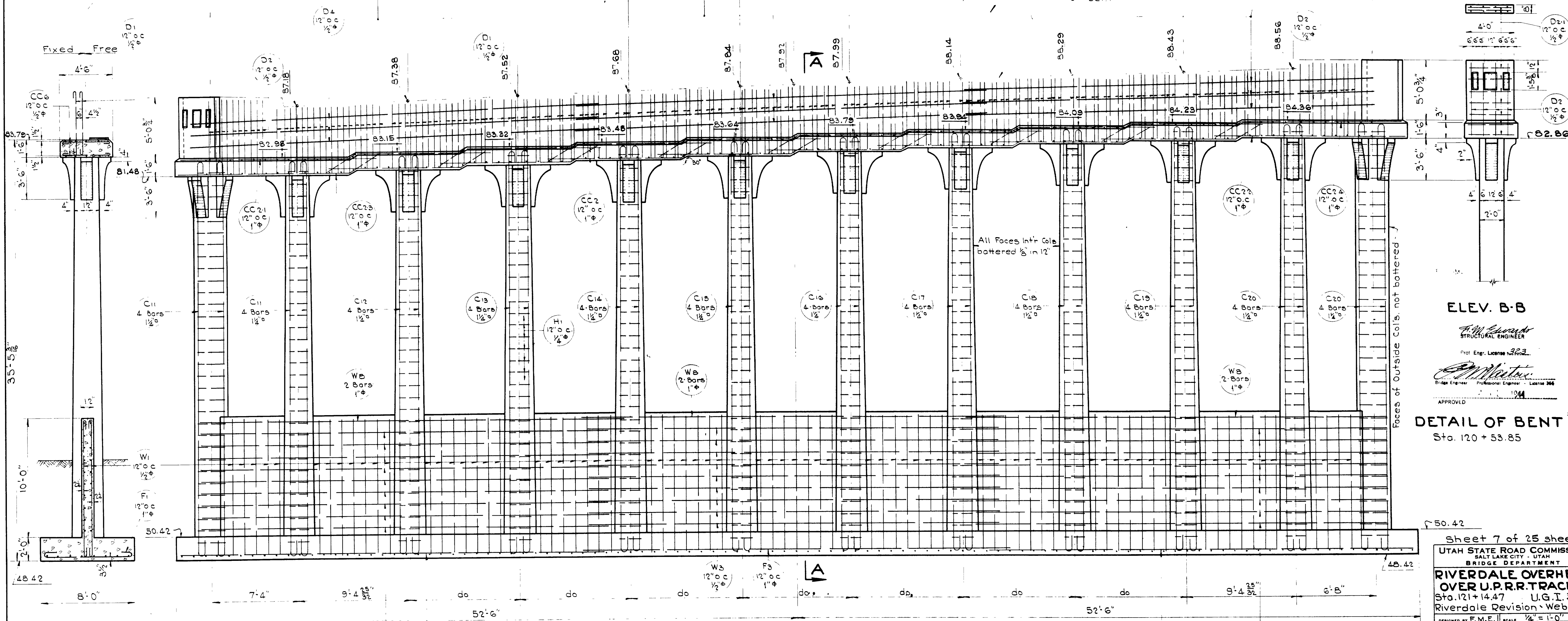
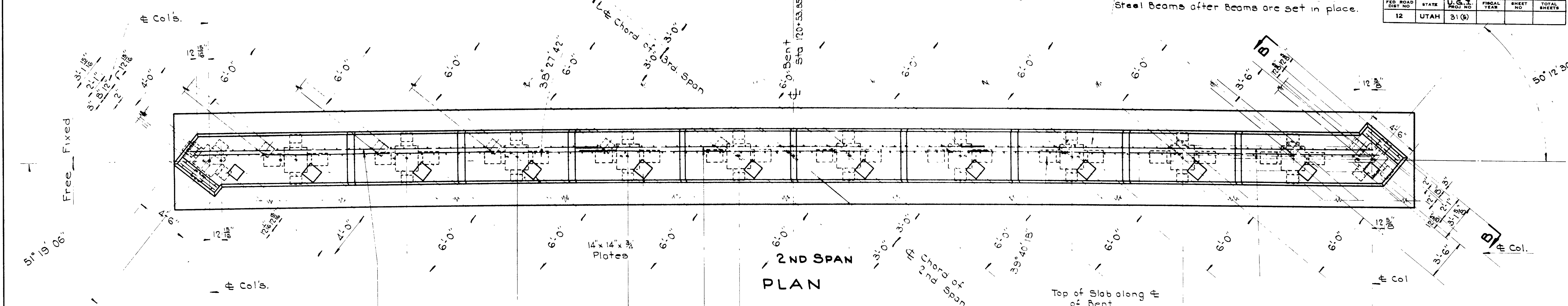
Sheet 6 of 25 sheets  
UTAH STATE ROAD COMMISSION  
SALT LAKE CITY - UTAH  
BRIDGE DEPARTMENT  
**RIVERDALE OVERHEAD  
OVER U.P.R.R. TRACKS**  
Sta. 121+14.47 U.G.I. 31(6)  
Riverdale Revision - Weber Co.  
DESIGNED BY: F.M.E. SCALE: As noted  
DRAWN BY: F.M.E. CHECKED BY: APPROVED BY: CHIEF ENGINEER  
NO. 29-14-7 DATE: D-485

REVISIONS	DATE	BY

3RD. SPAN; STRUCT. STEEL BEAMS

Pour End Diaphragms between Struct. Steel Beams after Beams are set in place.

FED. ROAD DIST. NO.	STATE	U.G.I. PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
12	UTAH	31 (6)			



REVISIONS	DATE	BY

SECTION A-A

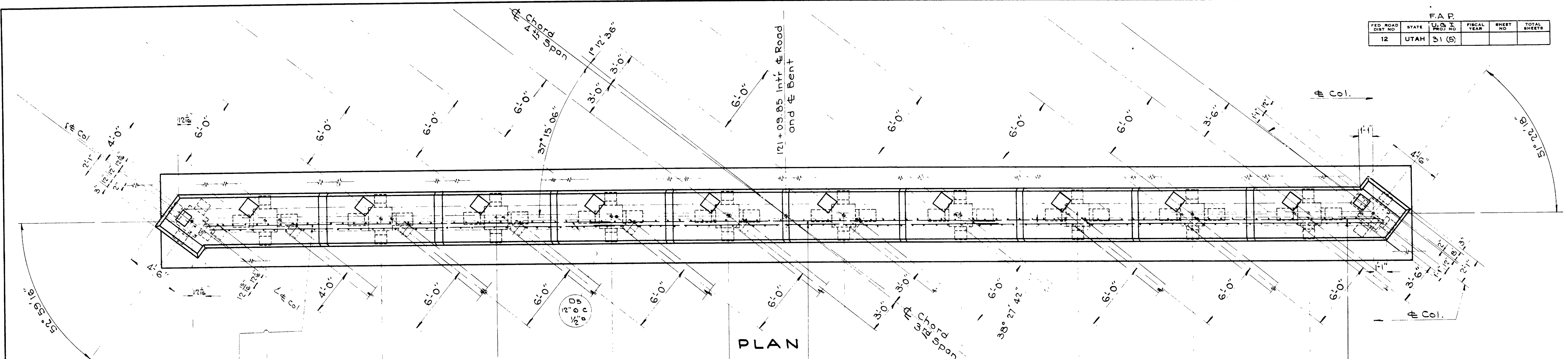
ELEVATION

ELEV. B-B  
 F.M.E. STRUCTURAL ENGINEER  
 Prof. Eng. License 262  
 APPROVED  
 DETAIL OF BENT #2  
 Sta. 120 + 53.85

Sheet 7 of 25 sheets  
 UTAH STATE ROAD COMMISSION  
 SALT LAKE CITY - UTAH  
 BRIDGE DEPARTMENT  
**RIVERDALE OVERHEAD  
 OVER U.P.R.R. TRACKS**  
 Sta. 121 + 14.47 U.G.I. 31(6)  
 Riverdale Revision - Weber Co.  
 DESIGNED BY F.M.E. SCALE 1/4" = 1'-0"  
 DRAWN BY F.M.E. ISSUED  
 CHECKED BY APPROVED CHIEF BRIDGE ENGINEER  
 EXAMINED BY  
 NO. 29-21-4-7 ORG. NO. D-485



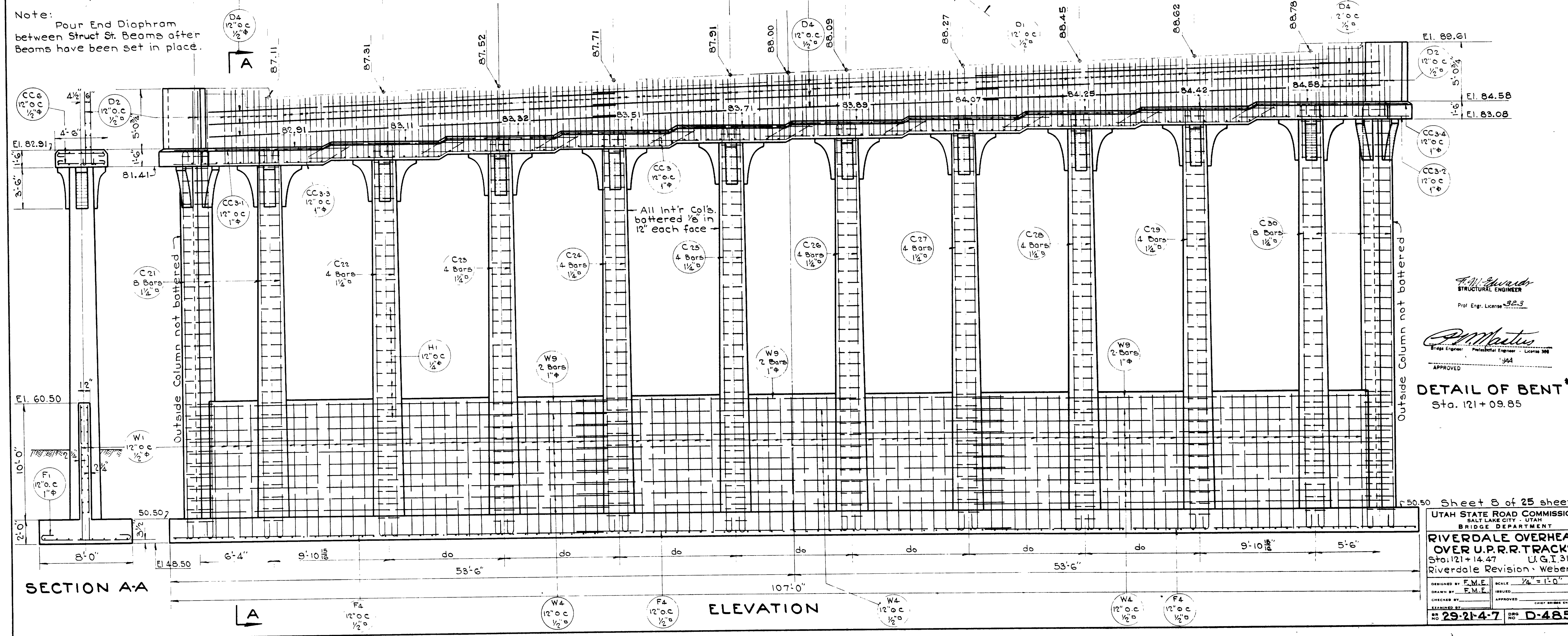
F.A.P.		FISCAL YEAR	SHEET NO.	TOTAL SHEETS
FED. ROAD DIST. NO.	STATE	UTAH	31 (5)	



PLAN

Note: Pour End Diaphragm between Struct St. Beams after Beams have been set in place.

REVISIONS	DATE	BY



SECTION A-A

ELEVATION

*F.W. Gardner*  
STRUCTURAL ENGINEER  
Prof. Engr. License 323

*AmMaster*  
Bridge Engineer - Professional Engineer - License 308  
1944

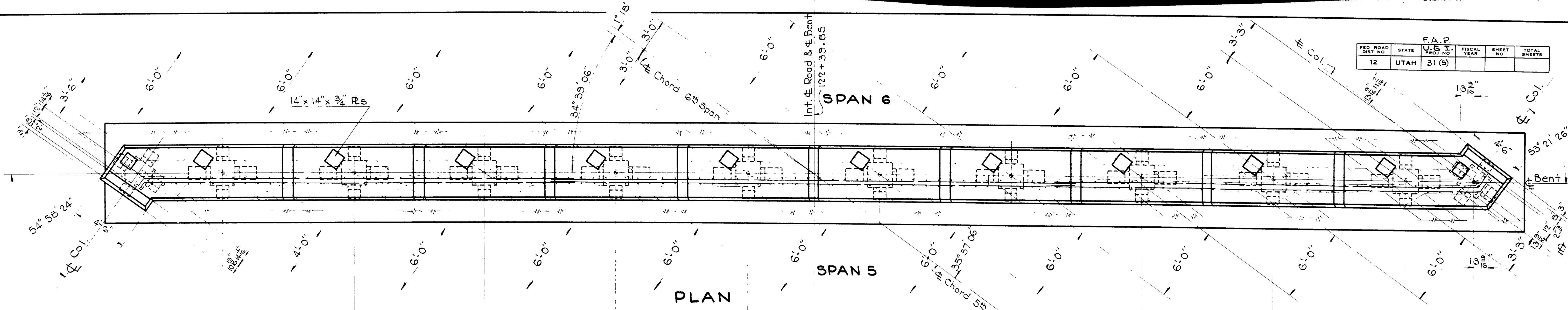
APPROVED  
**DETAIL OF BENT #3**  
Sta. 121+09.85

Sheet 8 of 25 sheets  
UTAH STATE ROAD COMMISSION  
SALT LAKE CITY - UTAH  
BRIDGE DEPARTMENT  
**RIVERDALE OVERHEAD**  
OVER U.P.R.R. TRACKS  
Sta. 121+14.47 U.G.I. 31(6)  
Riverdale Revision - Weber Co.  
DESIGNED BY F.M.E. SCALE 1/4" = 1'-0"  
DRAWN BY F.M.E. ISSUED  
CHECKED BY APPROVED  
BY NO. 29-214-7 DRG NO. D-485

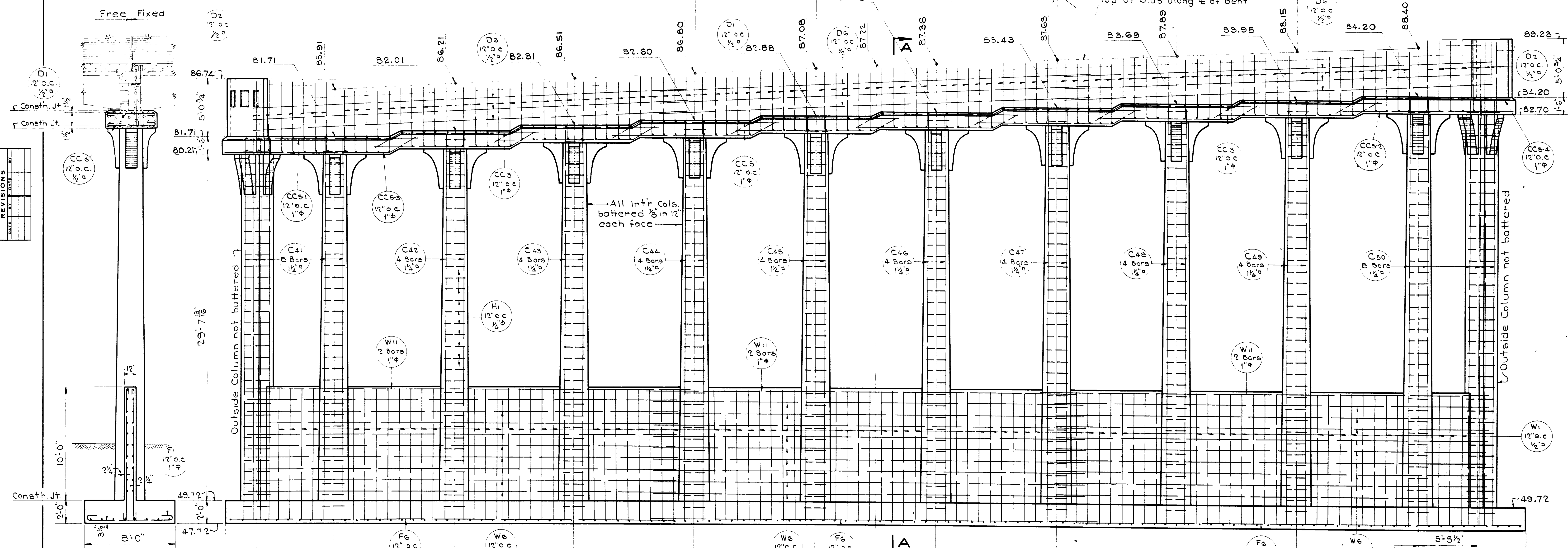




FED. ROAD DIST. NO.	STATE	F.A.P. U.S. 31(5)	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
12	UTAH	31(5)			



PLAN



ELEVATION

REVISIONS	DATE	BY

SECTION A-A

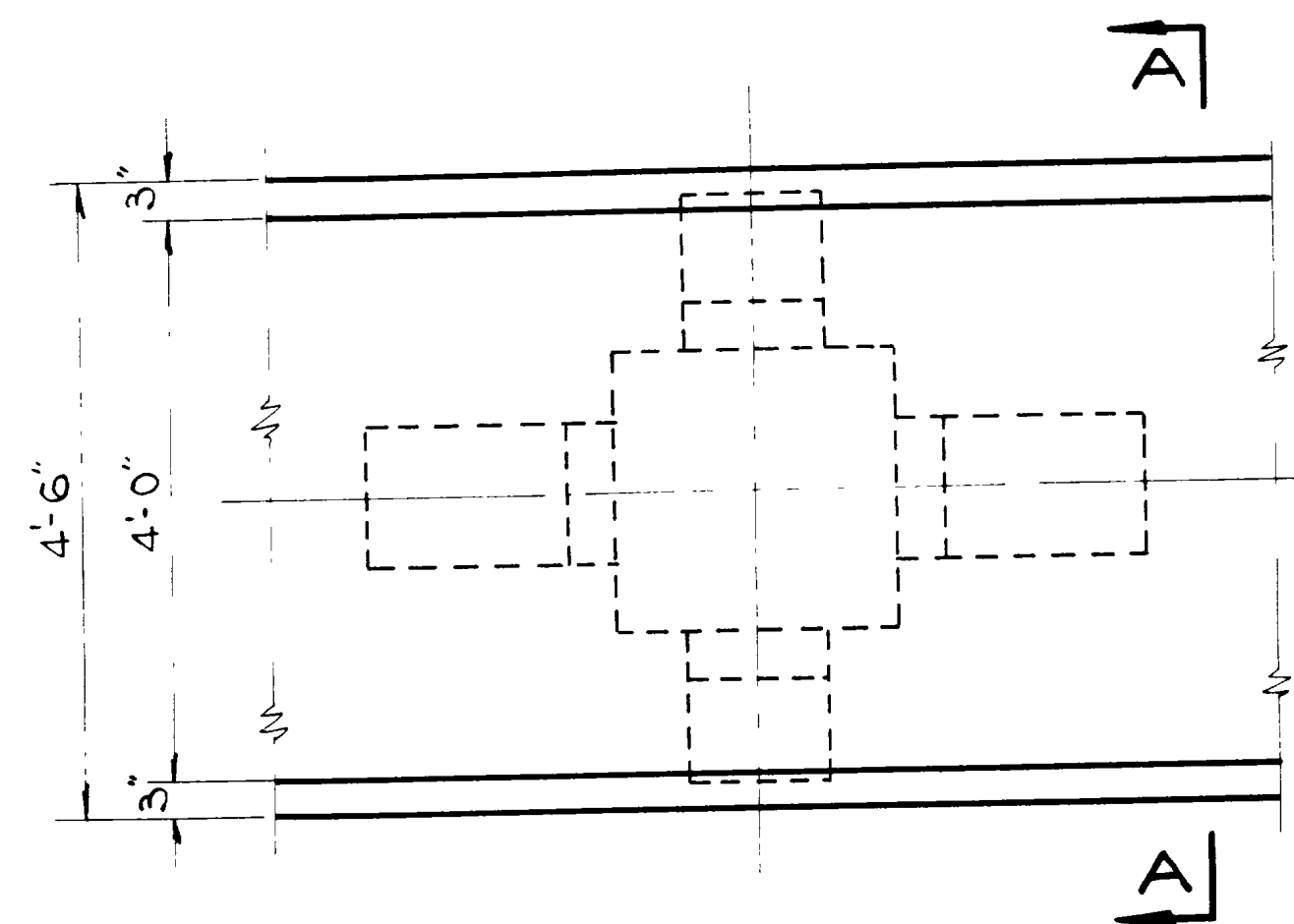
Sheet 10 of 25 sheets  
 UTAH STATE ROAD COMMISSION  
 SALT LAKE CITY - UTAH  
 BRIDGE DEPARTMENT  
**RIVERDALE OVERHEAD**  
**OVER U.P.R.R. TRACKS**  
 Sta. 121+14.47 U.G.I. 31(5)  
 Riverdale Revision - Weber &  
 Sta. 122 + 39.85

DESIGNED BY: F.M.E.  
 DRAWN BY: F.M.E.  
 CHECKED BY: F.M.E.  
 EXAMINED BY: F.M.E.  
 APPROVED: F.M.E.  
 SCALE: 1/4" = 1'-0"

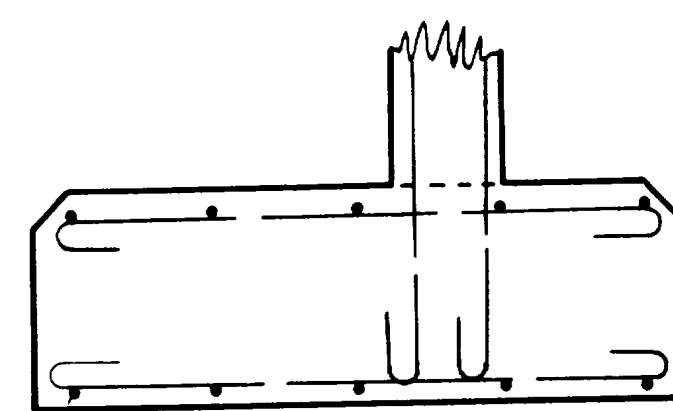
DESIGNED BY: F.M.E.	SCALE: 1/4" = 1'-0"
DRAWN BY: F.M.E.	ISSUED:
CHECKED BY: F.M.E.	APPROVED:
EXAMINED BY: F.M.E.	CHIEF BRIDGE ENGINEER:
NO. 29-21-4-7	DDO NO. D-485



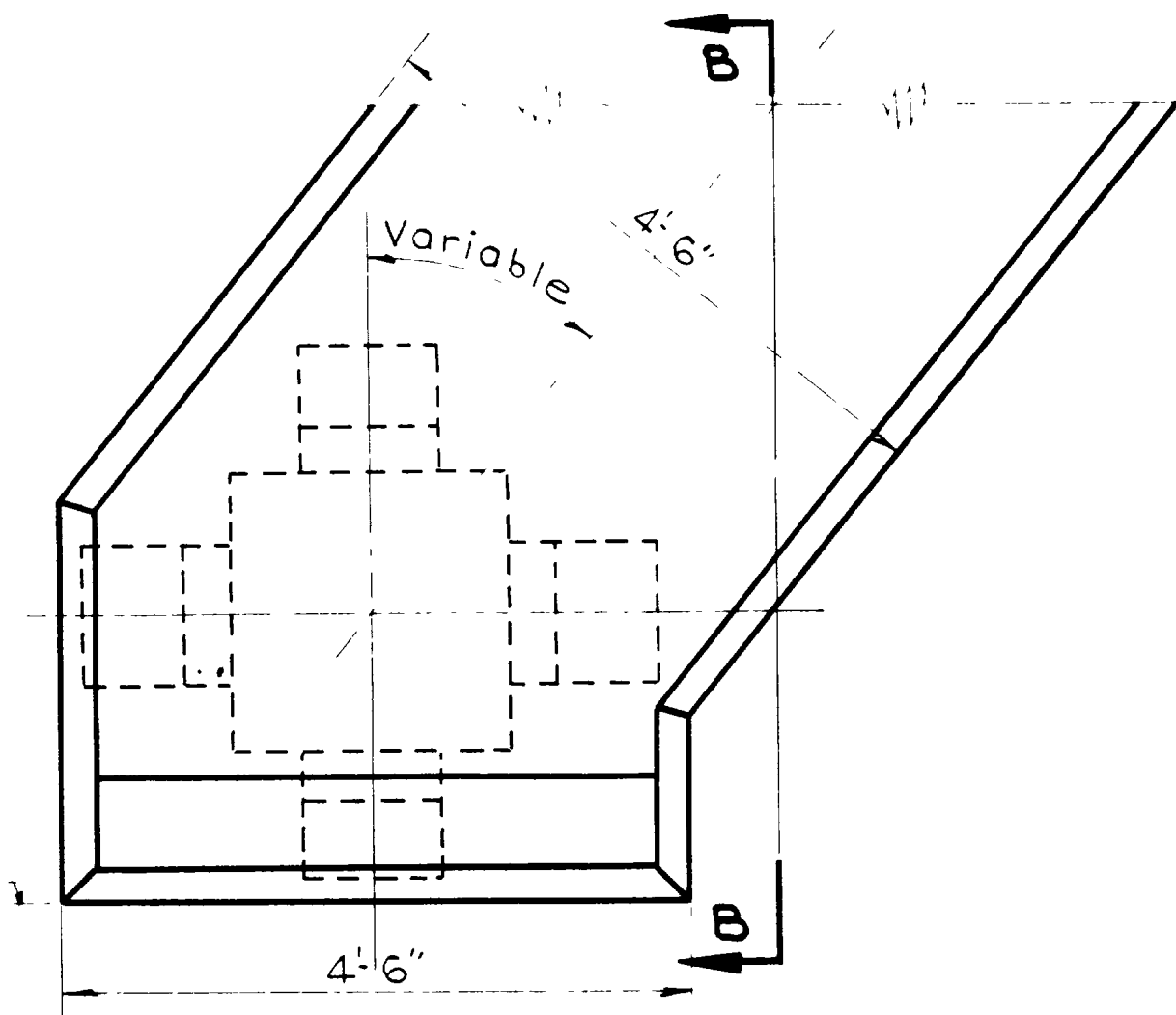
F.A.P.					
FED. ROAD DIST. NO.	STATE	U.S. PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
12	UTAH	31(5)			



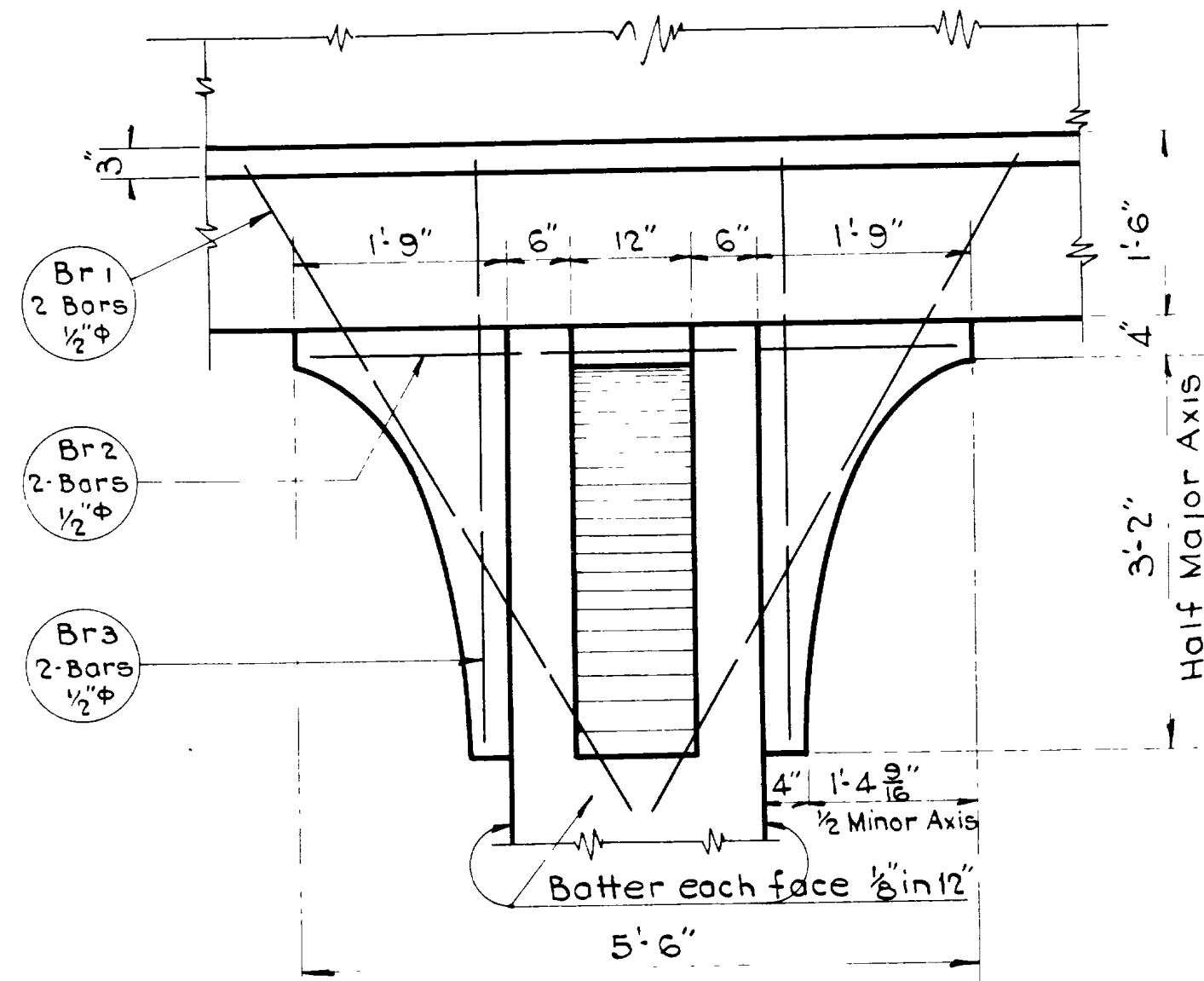
**PLAN**  
Diaphragm Wall not shown



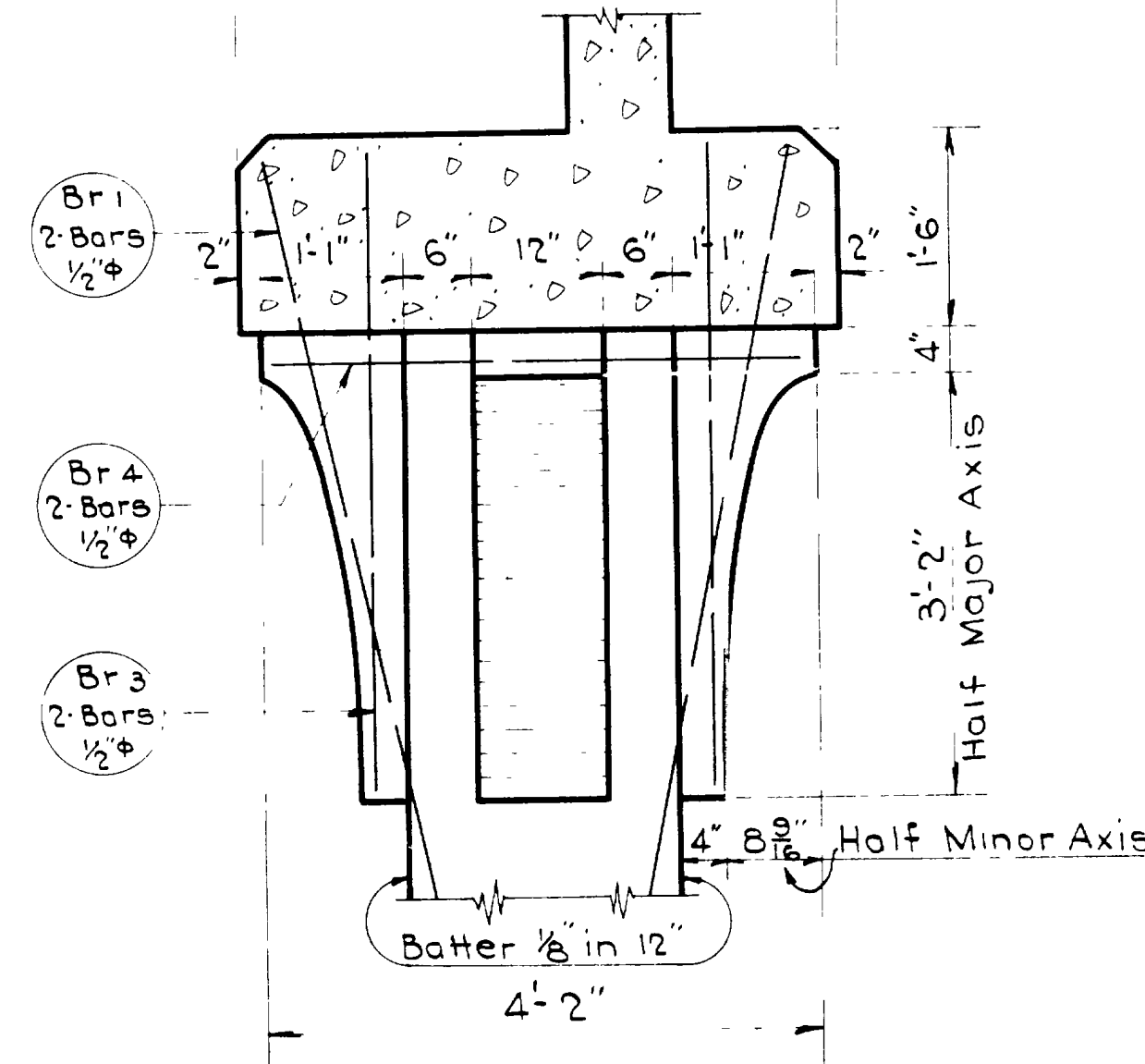
4'-6"



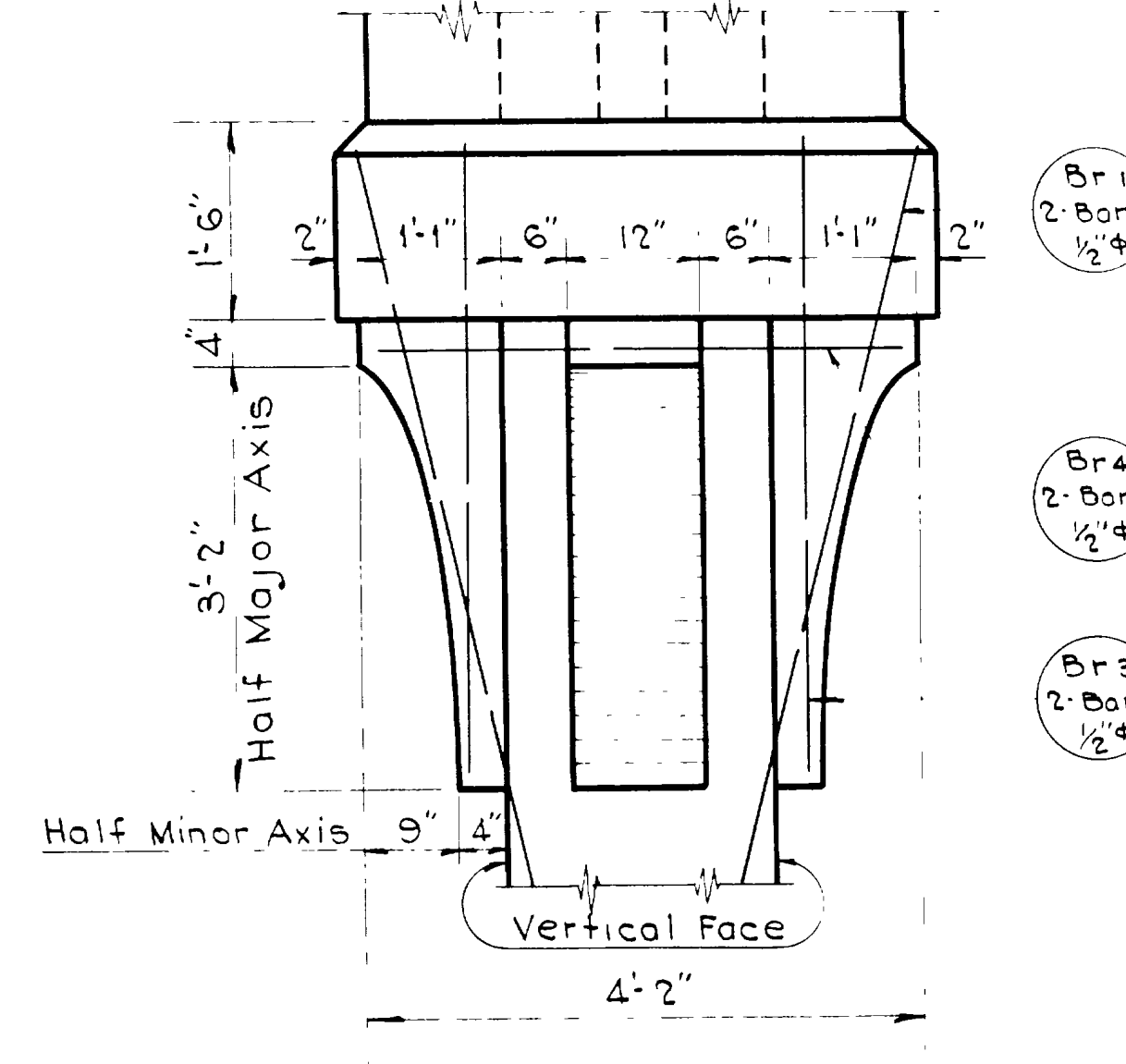
**PLAN**  
Diaphragm Wall not shown



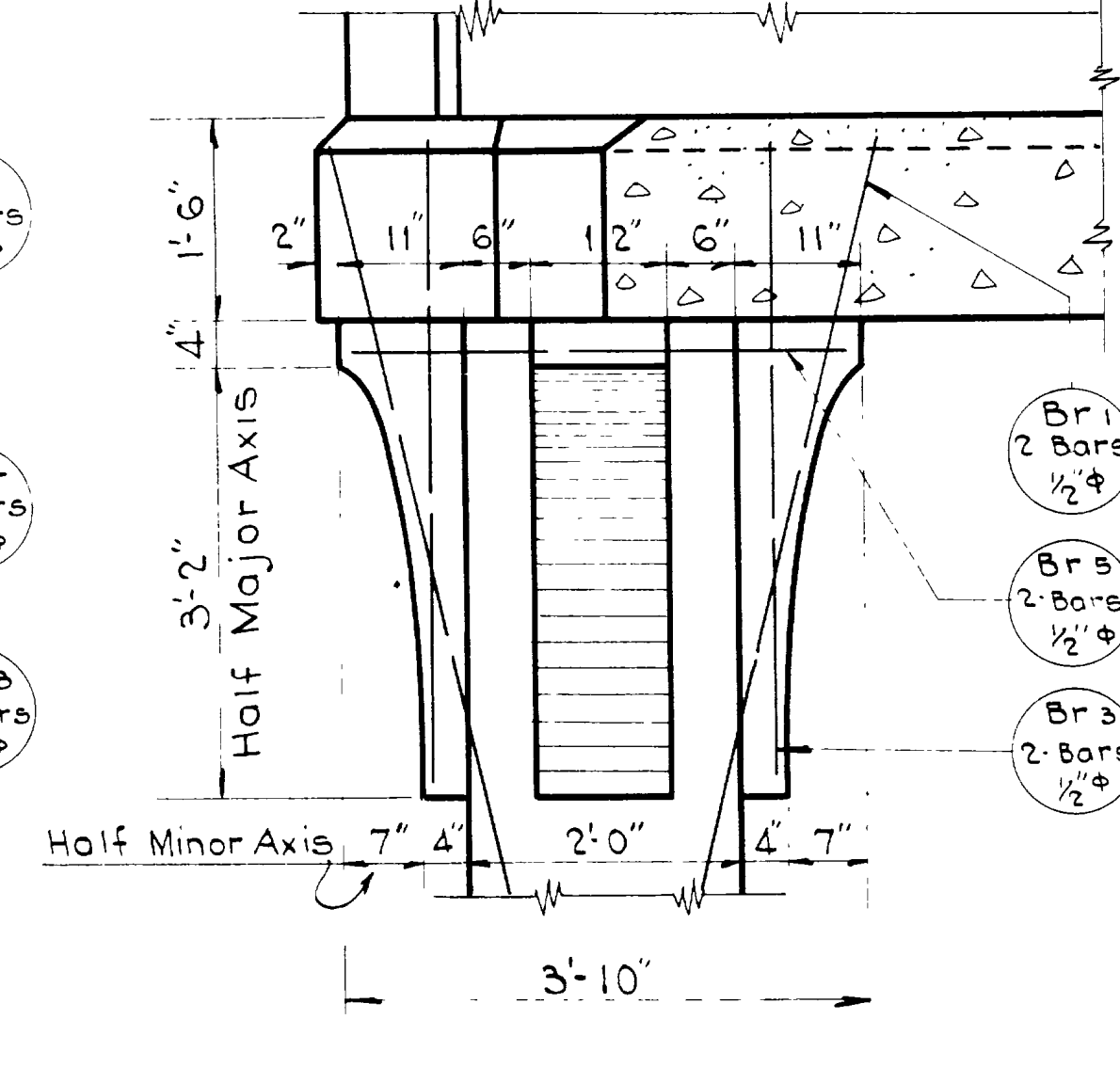
**ELEVATION**



**SECTION A-A**



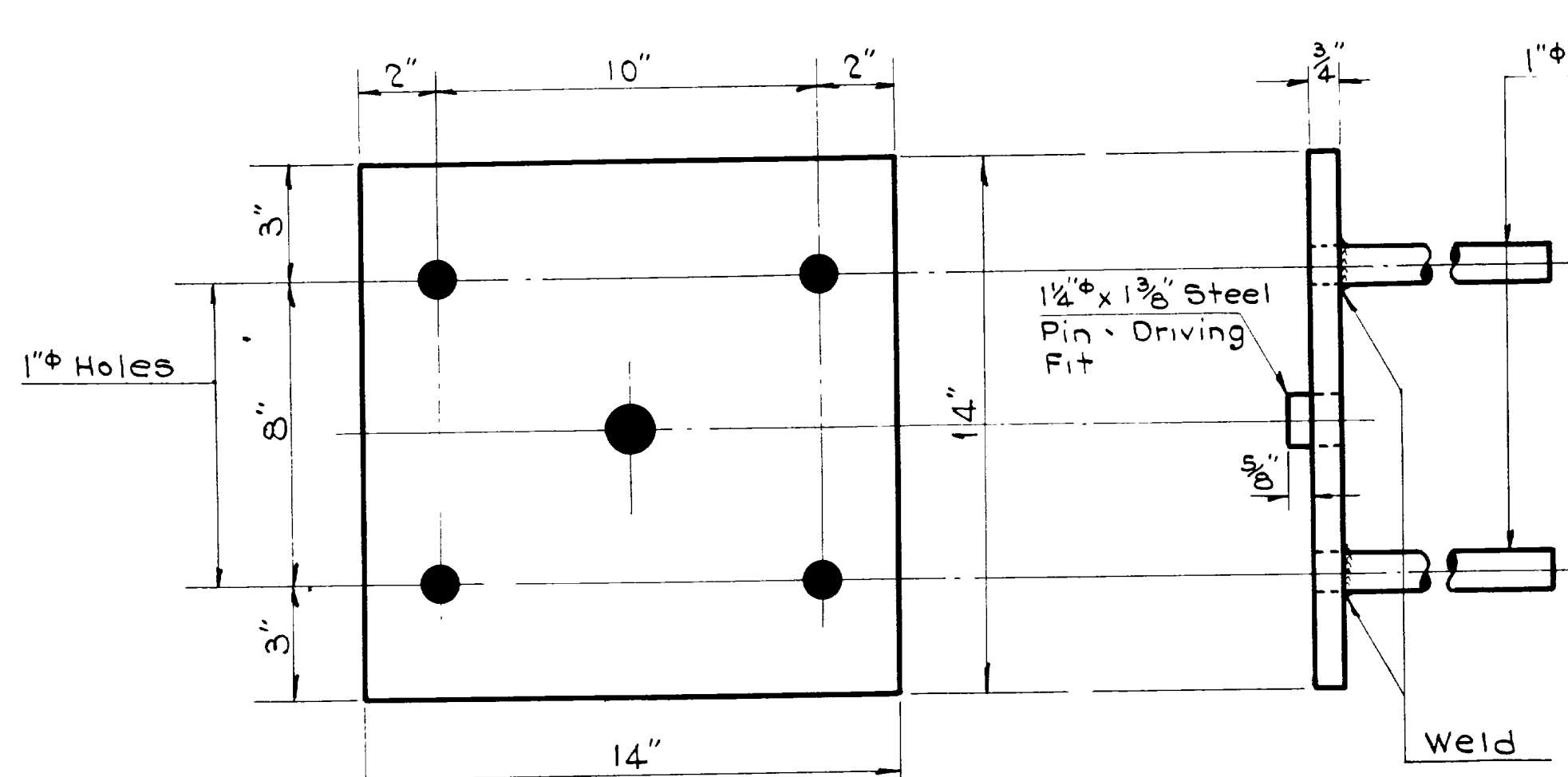
**ELEVATION**



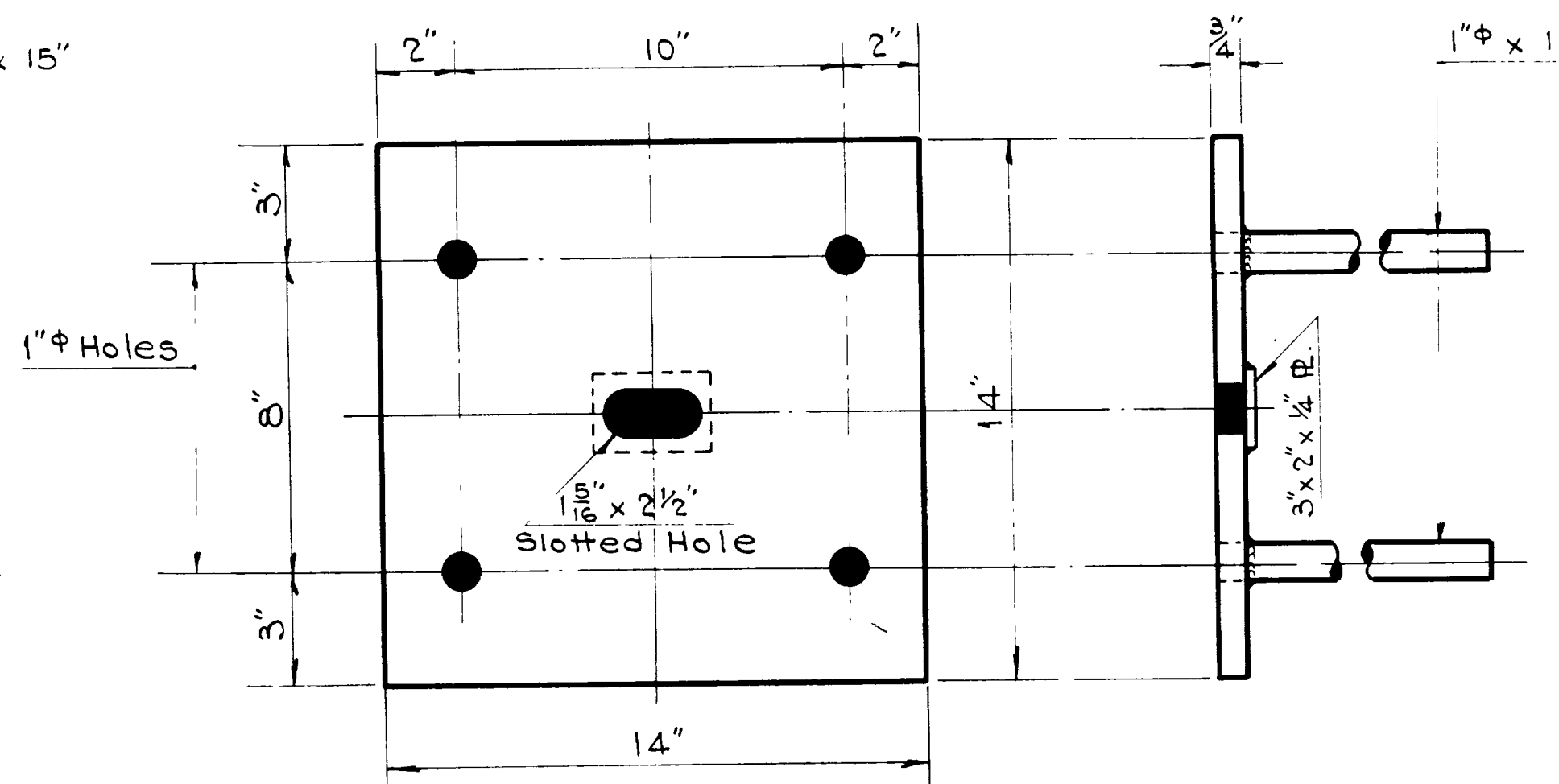
**SECTION B-B**

**DETAIL OF BRACKET FOR INTERIOR COLUMNS**  
Required Brackets for 50-Int'r. Columns Thus

**DETAIL OF BRACKET FOR OUTSIDE COLUMNS**  
Required Brackets for 10-Columns Thus

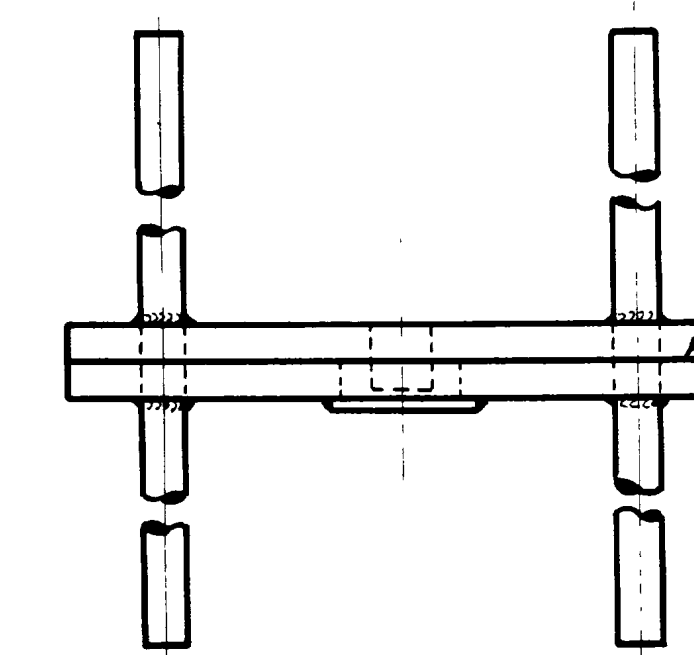


**DETAIL OF TOP PLATE - 50-REQ'D.**



**DETAIL OF BOTTOM PLATE - 50-REQ'D.**

**DETAIL OF STRUCTURAL STEEL EXP'N. UNIT FOR INTERIOR BEAMS**  
MAKE 50 COMPLETE UNITS AS SHOWN



**ASSEMBLED UNIT**

Lubricate between  
Bs. with Graphite & Oil  
Paste when placing.

**DETAILS**

Wt. of One Complete Unit = 111 Lbs. Wt. of 50-Units = 5550 Lbs.

Sheet 11 of 25 sheets

UTAH STATE ROAD COMMISSION  
SALT LAKE CITY - UTAH  
BRIDGE DEPARTMENT

**RIVERDALE OVERHEAD  
OVER U.P.R.R. TRACKS**  
Sta. 121+14.47 U.G.I.31(5)  
Riverdale Revision - Weber Co.

DESIGNED BY: F.M.E. SCALE: 3/4" & 3" = 1'-0"  
DRAWN BY: F.M.E. ISSUED:  
CHECKED BY: APPROVED: CHIEF ENGINEER

NO. 29-21-4-7 DRG. NO. D-485

*F.M. Edwards*  
STRUCTURAL ENGINEER

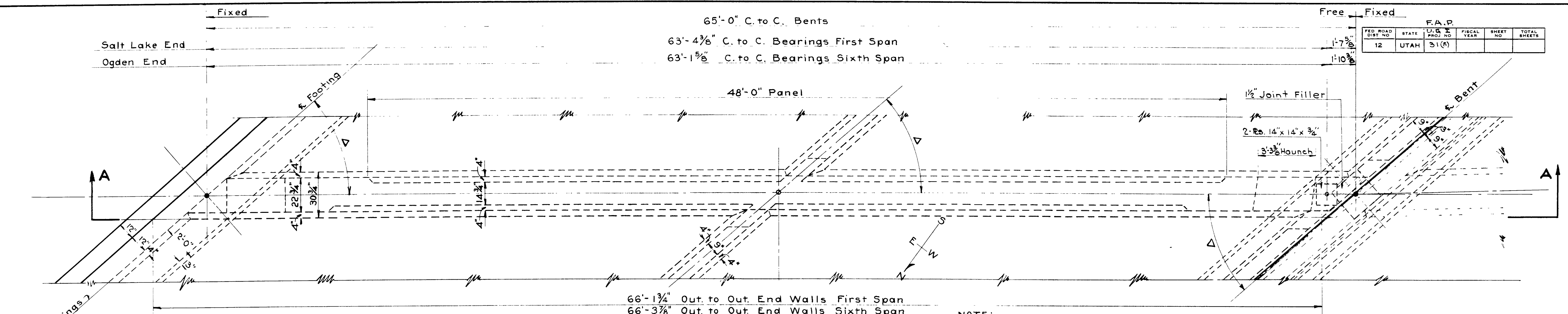
Prof. Engr. License #323

*F.M. Edwards*  
MAE 1944

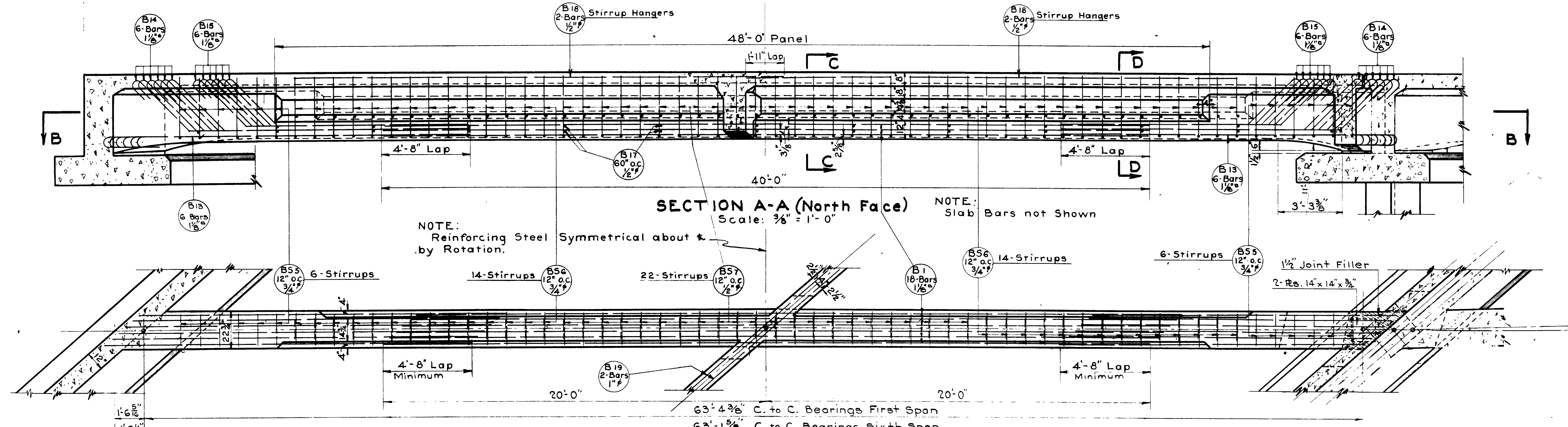
APPROVED

REVISIONS	DATE	BY

FED. ROAD DIST. NO.		STATE	F.A.P. PROJ. NO.		FISCAL YEAR	SHEET NO.	TOTAL SHEETS
12		UTAH	31(6)				



**PLAN OF BEAMS APPROACH SPANS**  
Scale: 3/8" = 1'-0"

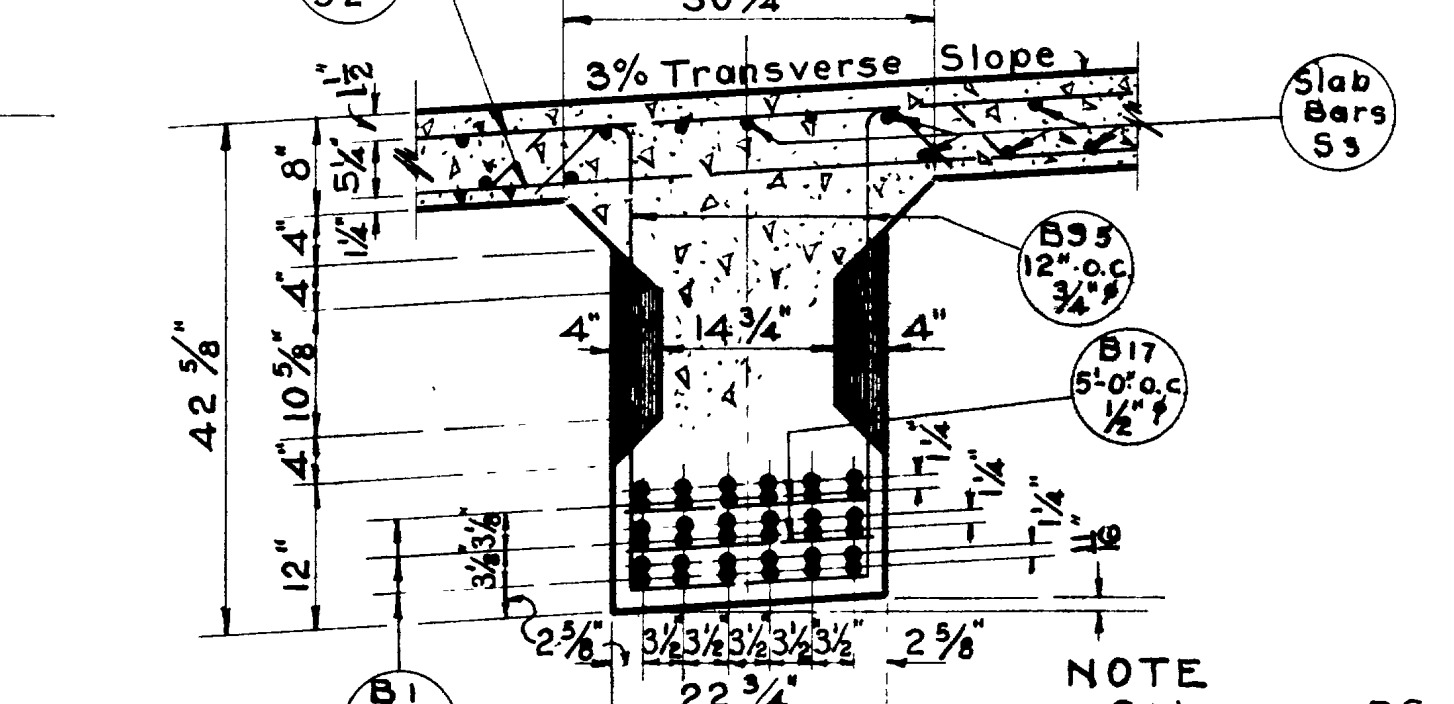


**SECTION A-A (North Face)**  
Scale: 3/8" = 1'-0"

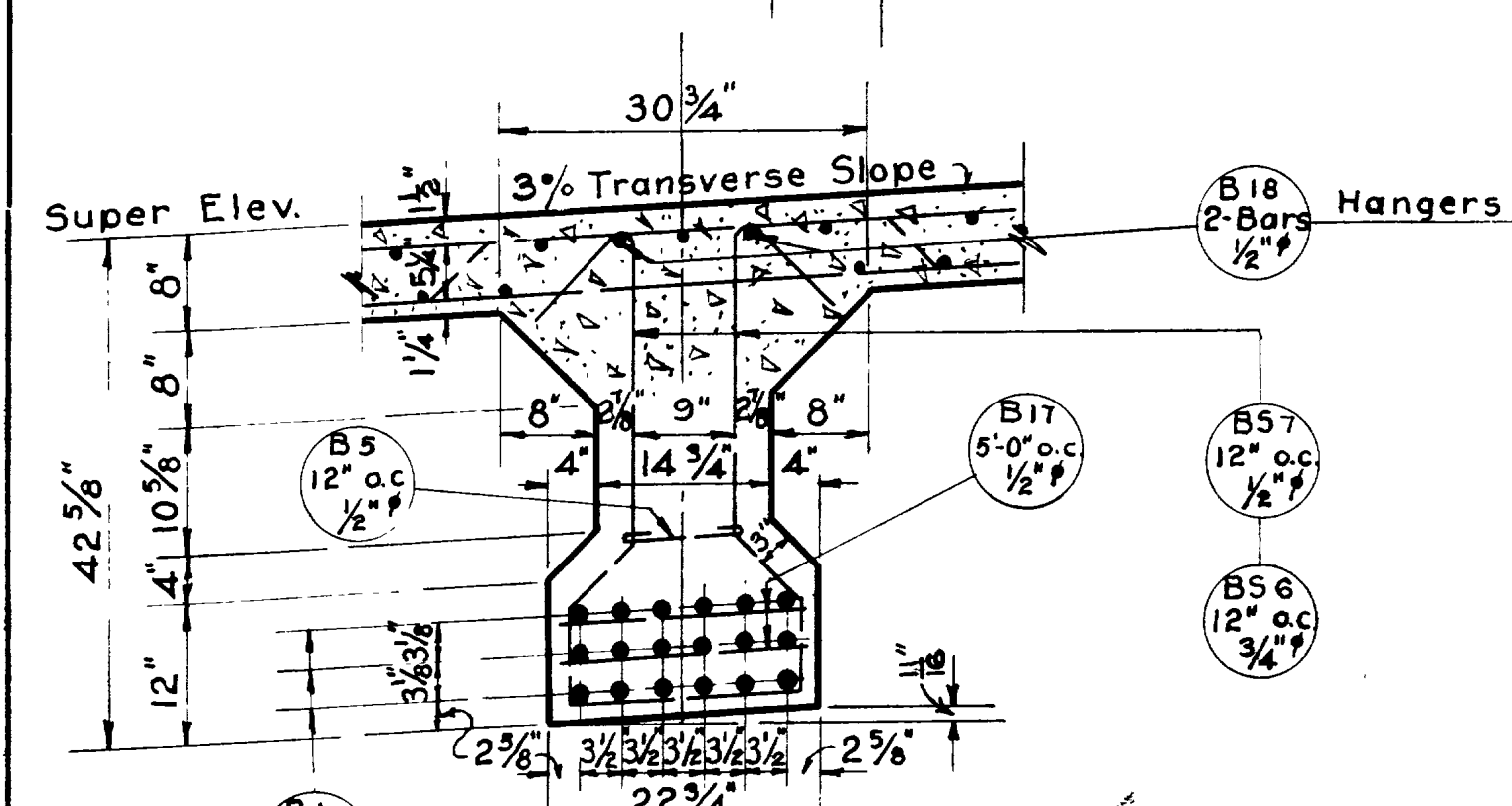
NOTE: Reinforcing Steel Symmetrical about  $\star$  by Rotation.

NOTE: Slab Bars not Shown

**SECTION B-B, REINFORCING STEEL IDENTICAL FOR 20 BEAMS AS SHOWN & NOTED**  
Scale: 3/8" = 1'-0"



NOTE: Stirrups B56 not Shown



**SECTION C-C**  
Scale: 3/4" = 1'-0"

REVISIONS	DATE	BY

**BEAM STEEL APPROACH SPANS NO'S. 1 & 6**

Sheet 12 of 25 Sheets

UTAH STATE ROAD COMMISSION  
SALT LAKE CITY - UTAH  
BRIDGE DEPARTMENT

**RIVERDALE OVERHEAD OVER U.P.R.R. TRACKS**  
Sta. 12+14.47 U.S.I. 31(6)  
Riverdale Revision: Weber

DESIGNED BY: F.M.E. SCALE: As noted  
DRAWN BY: F.M.E. ISSUED  
CHECKED BY: APPROVED  
EXAMINED BY: CHIEF BRIDGE ENGINEER

BR. NO. 29-214-7 Dwg. No. D-485

*F.M.E. Weber*  
STRUCTURAL ENGINEER  
Prof. Eng. License No. 5253

*P. Martin*



2nd Span 11'  
4th Span 12'  
5th Span 12'

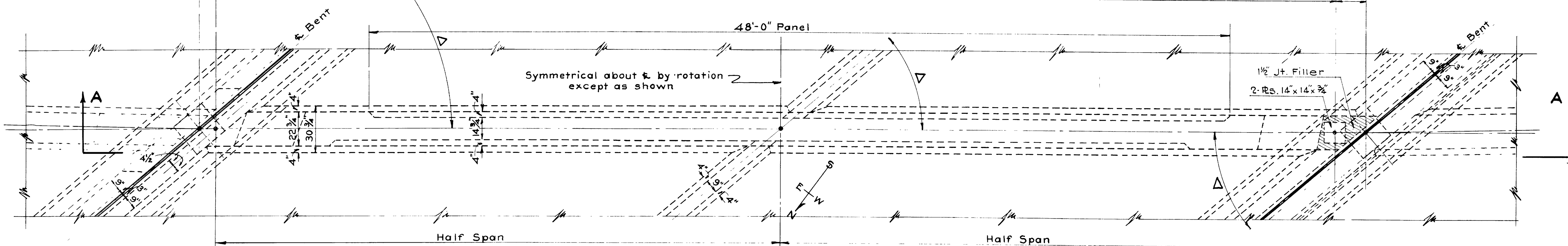
Fixed End

65'-0" C. to C. Bents

62'-4 1/8" C. to C. Bearings Second Span  
62'-2 1/2" C. to C. Bearings Fourth Span  
62'-1 1/8" C. to C. Bearings Fifth Span

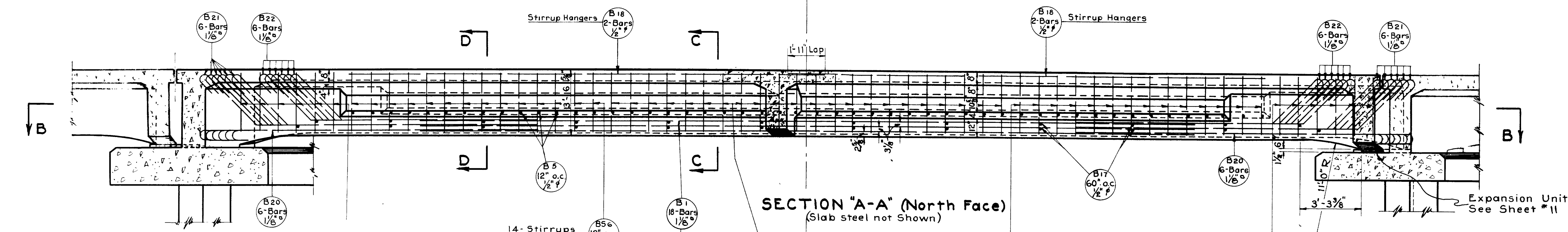
Free End 1'-8 1/2"  
1'-9 1/8"  
1'-9 1/8"

F.A.P.					
FED. ROAD DIST. NO.	STATE	U.G.I. PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
12	UTAH	31 (5)			

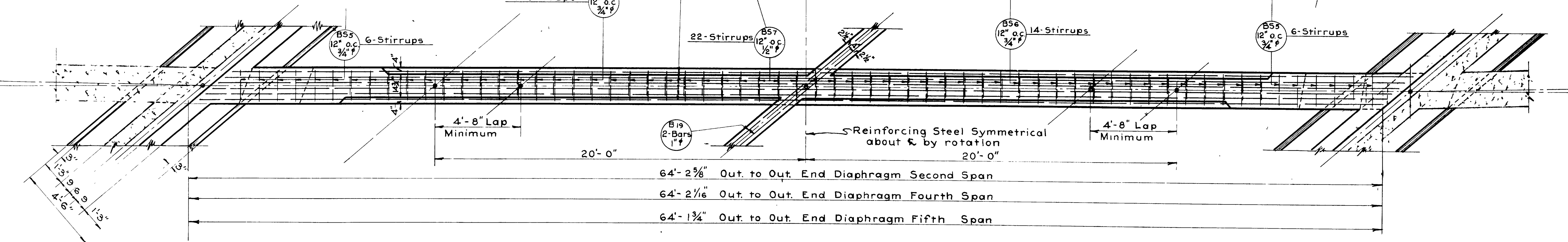


△ Second Span = 39°-40'-18"  
△ Fourth Span = 37°-15'-06"  
△ Fifth Span = 35°-57'-06"

PLAN OF BEAMS 2ND, 4TH, & 5TH SPANS



SECTION "A-A" (North Face)  
(Slab steel not shown)



SECTION "B-B"

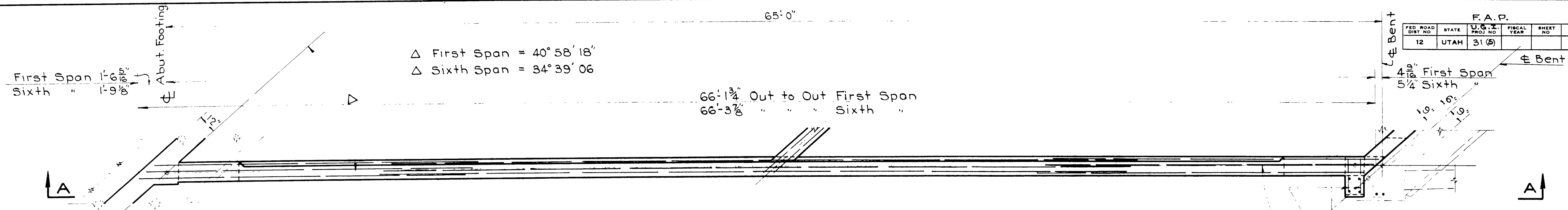
Reinforcing Steel Identical for 30 Beams as shown and noted. For Sections "C-C" & "D-D" See Sheet 12

BEAM STEEL 2ND, 4TH & 5TH SPANS

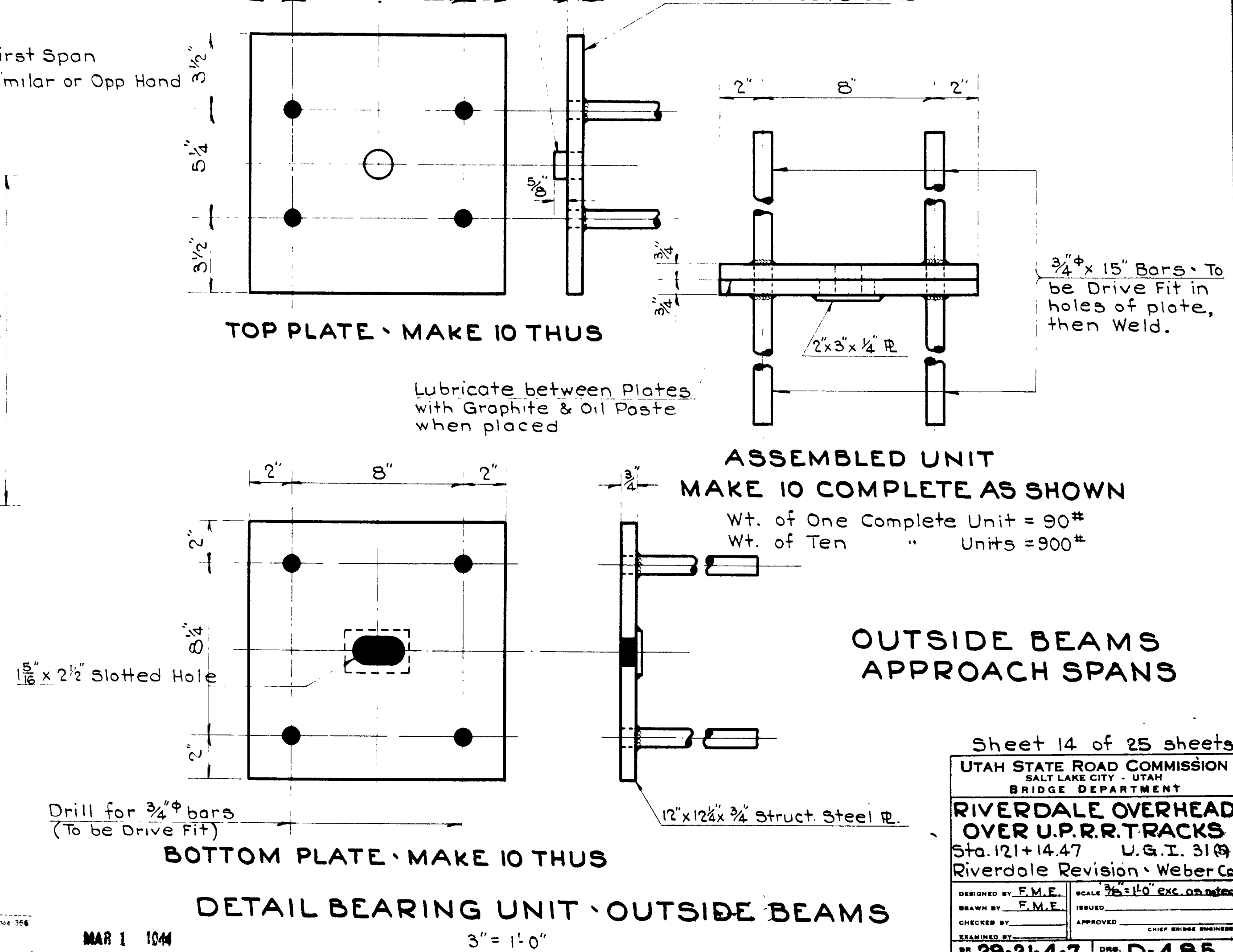
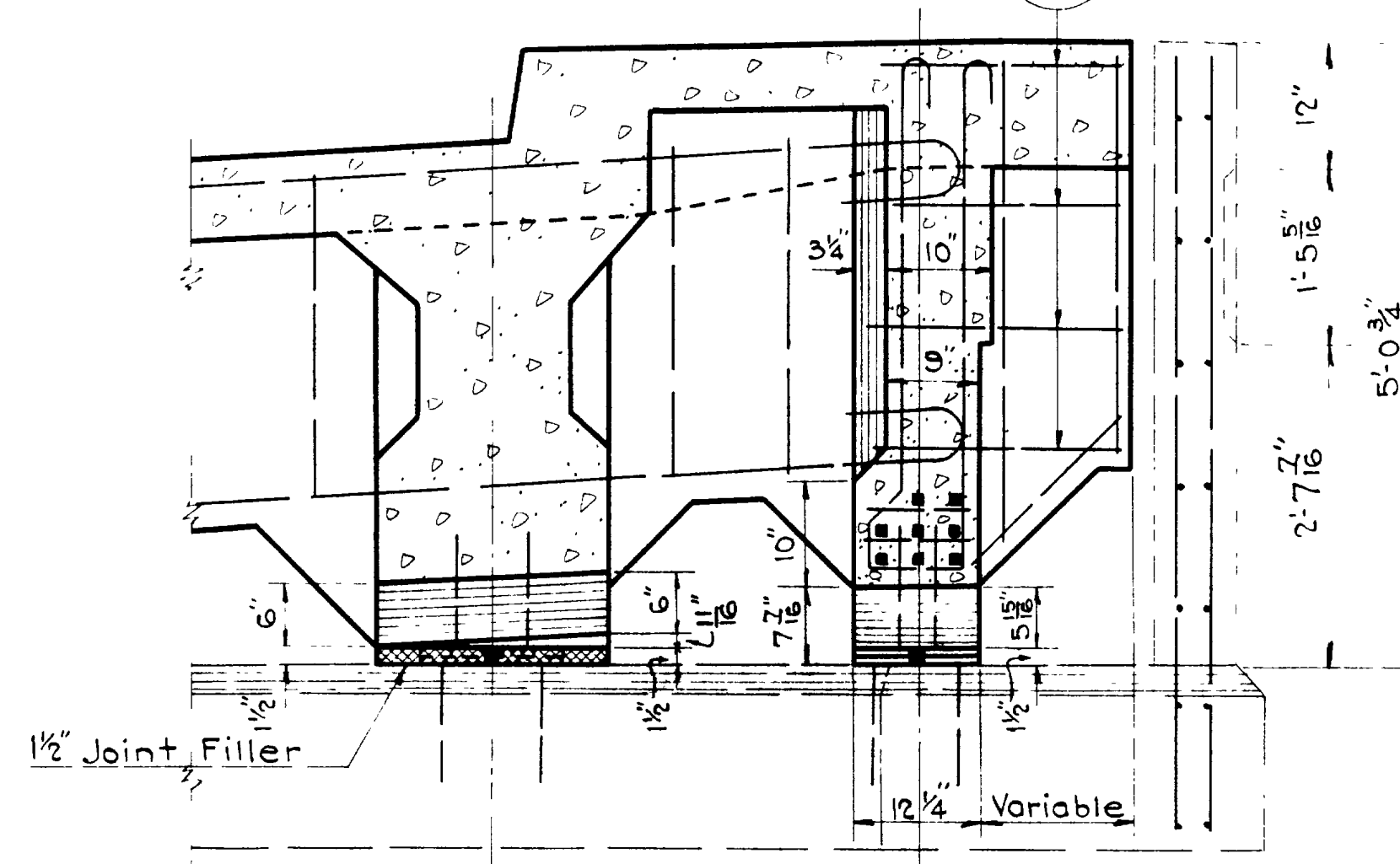
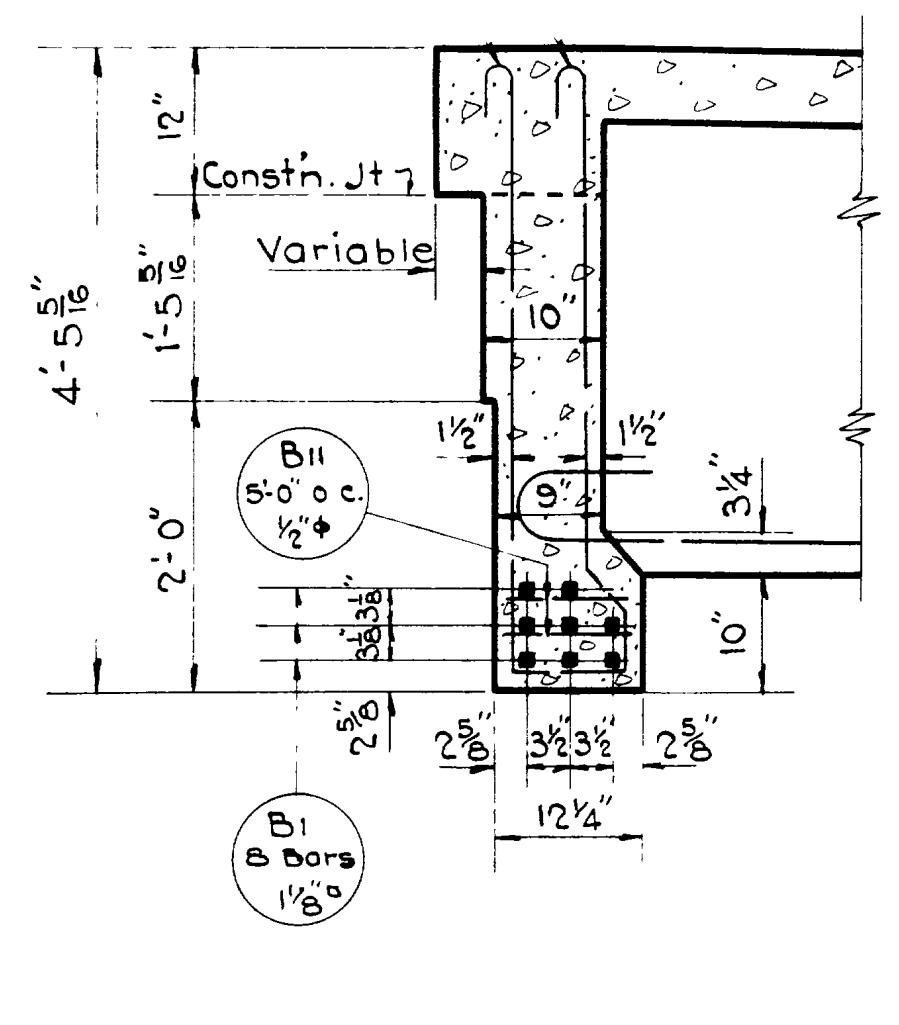
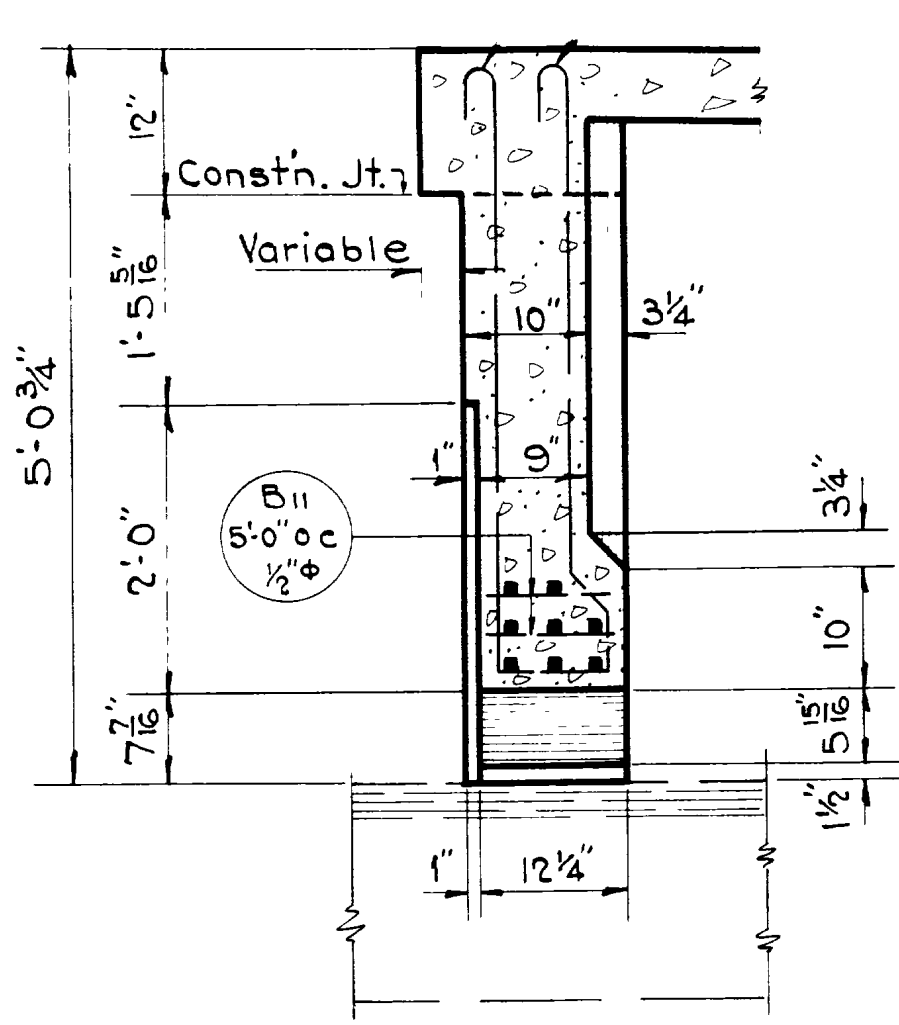
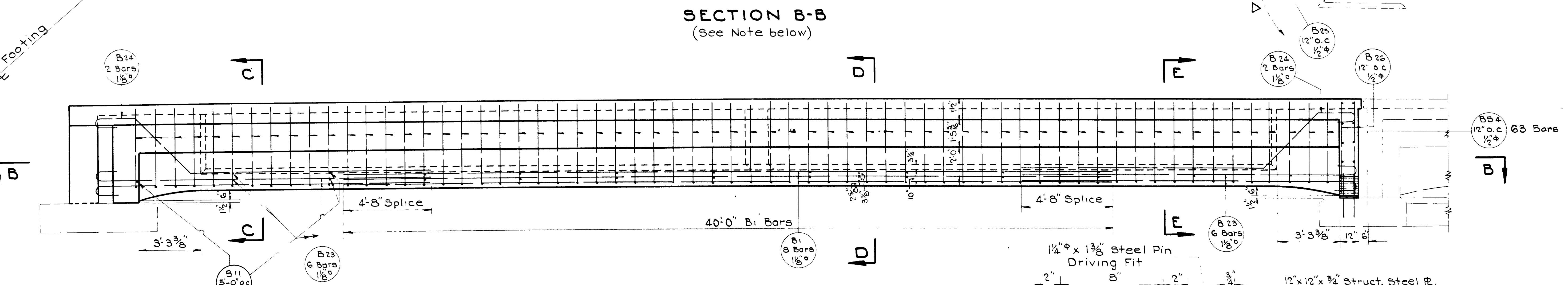
REVISIONS	DATE	BY	NO.

Sheet 13 of 25 sheets  
UTAH STATE ROAD COMMISSION  
SALT LAKE CITY - UTAH  
BRIDGE DEPARTMENT  
**RIVERDALE OVERHEAD OVER U.P.R.R. TRACKS**  
Sta. 121+14.47 U.G.I. No. 31  
Riverdale Revision No. 1  
Scale: 3/4" = 1'-0"  
DESIGNED BY: F.M. [Signature]  
DRAWN BY: [Signature]  
CHECKED BY: [Signature]  
DATE: 12-1-47  
No. 29-21-4-7 D-47

F.A.P.					
FED. ROAD DIST. NO.	STATE	U.S.C. PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
12	UTAH	31 (S)			



REVISIONS	DATE	BY



Sheet 14 of 25 sheets

UTAH STATE ROAD COMMISSION  
SALT LAKE CITY, UTAH  
BRIDGE DEPARTMENT

**RIVERDALE OVERHEAD  
OVER U.P.R.R. TRACKS**

Sta. 121+14.47 U.S.I. 310

Riverdale Revision - Weber Co.

DESIGNED BY: F.M.E. SCALE: 3/8" = 1'-0" EXC. AS NOTED

DRAWN BY: F.M.E. ISSUED

CHECKED BY: APPROVED

BY: F.M.E. DATE: 29-21-47

DR. NO. D-485

323

F.M. Weber  
STRUCTURAL ENGINEER

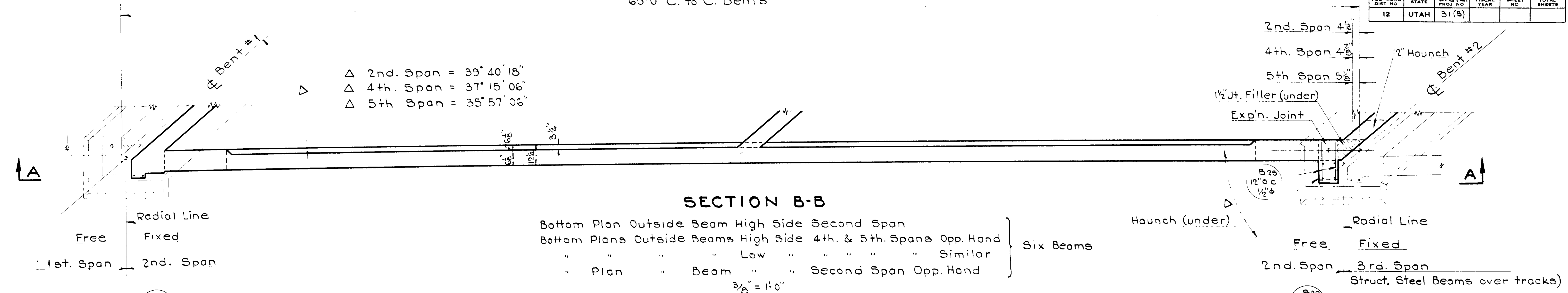
MAR 1 1944



65'-0" C. to C. Bents

F.A.P.					
FED. ROAD DIST. NO.	STATE	U.S. PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
12	UTAH	31(B)			

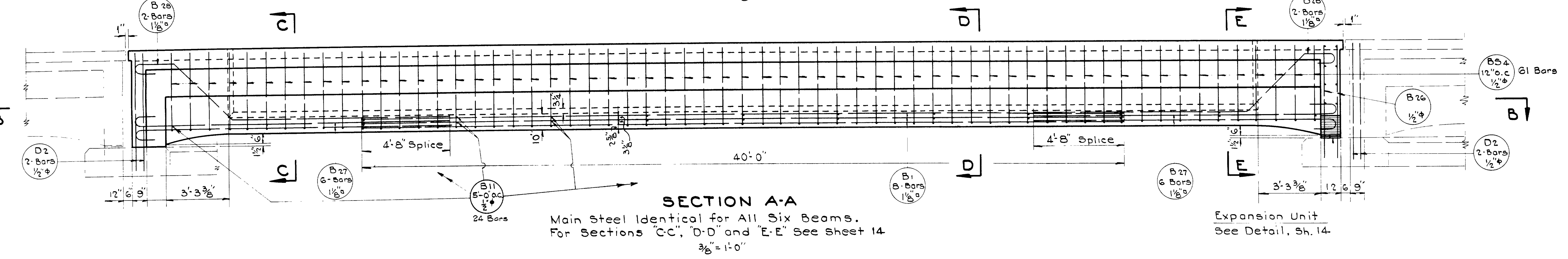
$\Delta$  2nd. Span = 39° 40' 18"  
 $\Delta$  4th. Span = 37° 15' 06"  
 $\Delta$  5th Span = 35° 57' 06"



**SECTION B-B**

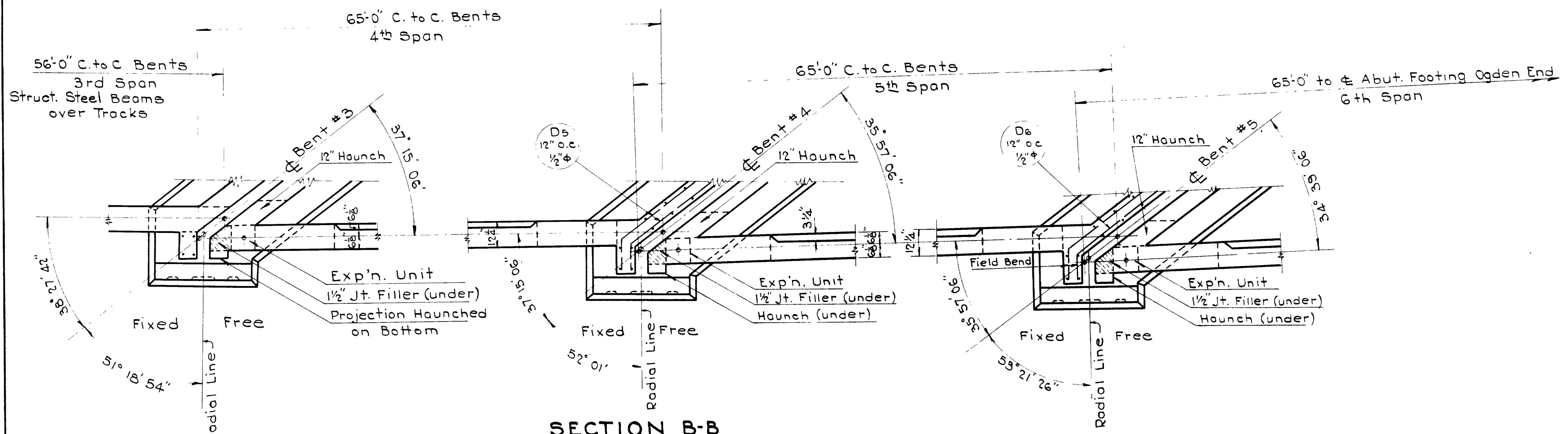
Bottom Plan Outside Beam High Side Second Span  
 Bottom Plans Outside Beams High Side 4th. & 5th. Spans Opp. Hand  
 " " " " " " " " Similar  
 " Plan " Beam " " Second Span Opp. Hand  
 3/8" = 1'-0"

REVISIONS	DATE	BY



**SECTION A-A**

Main Steel Identical for All Six Beams.  
 For Sections "C-C", "D-D" and "E-E" See Sheet 14.  
 3/8" = 1'-0"



**SECTION B-B**

Bottom Plan Outside Beams High Side 4th. & 5th. Spans  
 " " " " " " " " (Opp. Hand)  
 3/8" = 1'-0"

**OUTSIDE BEAMS  
2ND, 4TH & 5TH SPANS**

Sheet 15 of 25 sheets

UTAH STATE ROAD COMMISSION  
SALT LAKE CITY - UTAH  
BRIDGE DEPARTMENT

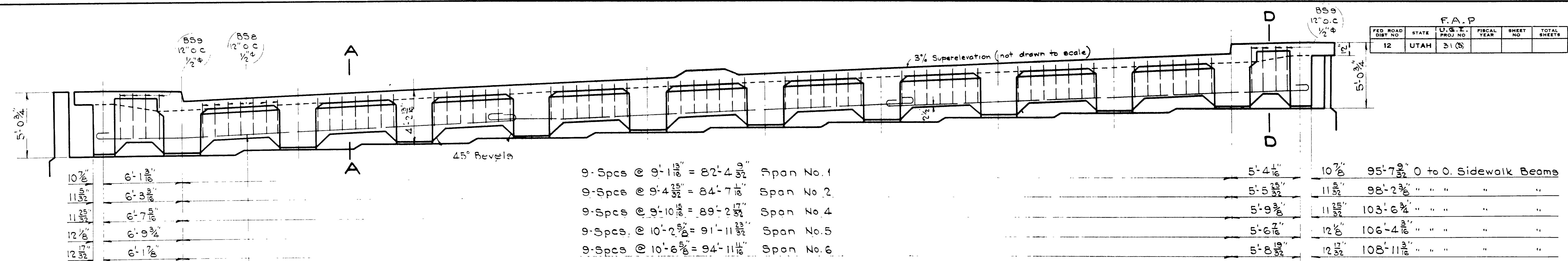
**RIVERDALE OVERHEAD  
OVER U.P.R.R. TRACKS**  
Sta. 121+14.47 U.G.I. 31(B)  
Riverdale Revision - Weber Co

DESIGNED BY: F.M.E. SCALE: 3/8" = 1'-0"  
DRAWN BY: F.M.E. ISSUED:  
CHECKED BY: APPROVED:  
EXAMINED BY: CHIEF BRIDGE ENGINEER

APPROVED: [Signature] DATE: MAR 1 1944

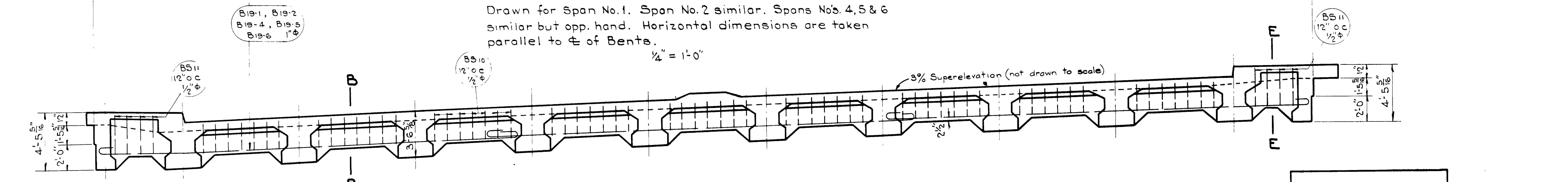
NO. 29-21-4-7 DRG. NO. D-485

FED. ROAD DIST. NO.	STATE	U.S.I. PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
12	UTAH	31(S)			



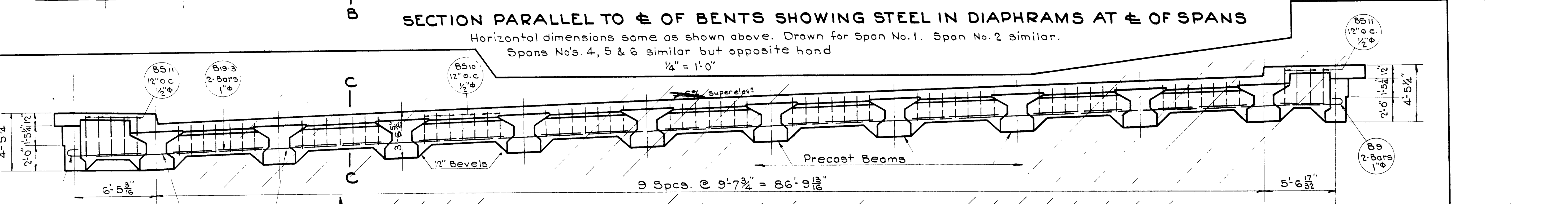
**SECTION ALONG FACE OF END DIAPHRAMS**

Drawn for Span No. 1. Span No. 2 similar. Spans No's. 4, 5 & 6 similar but opp. hand. Horizontal dimensions are taken parallel to  $\phi$  of Bents.  
1/4" = 1'-0"



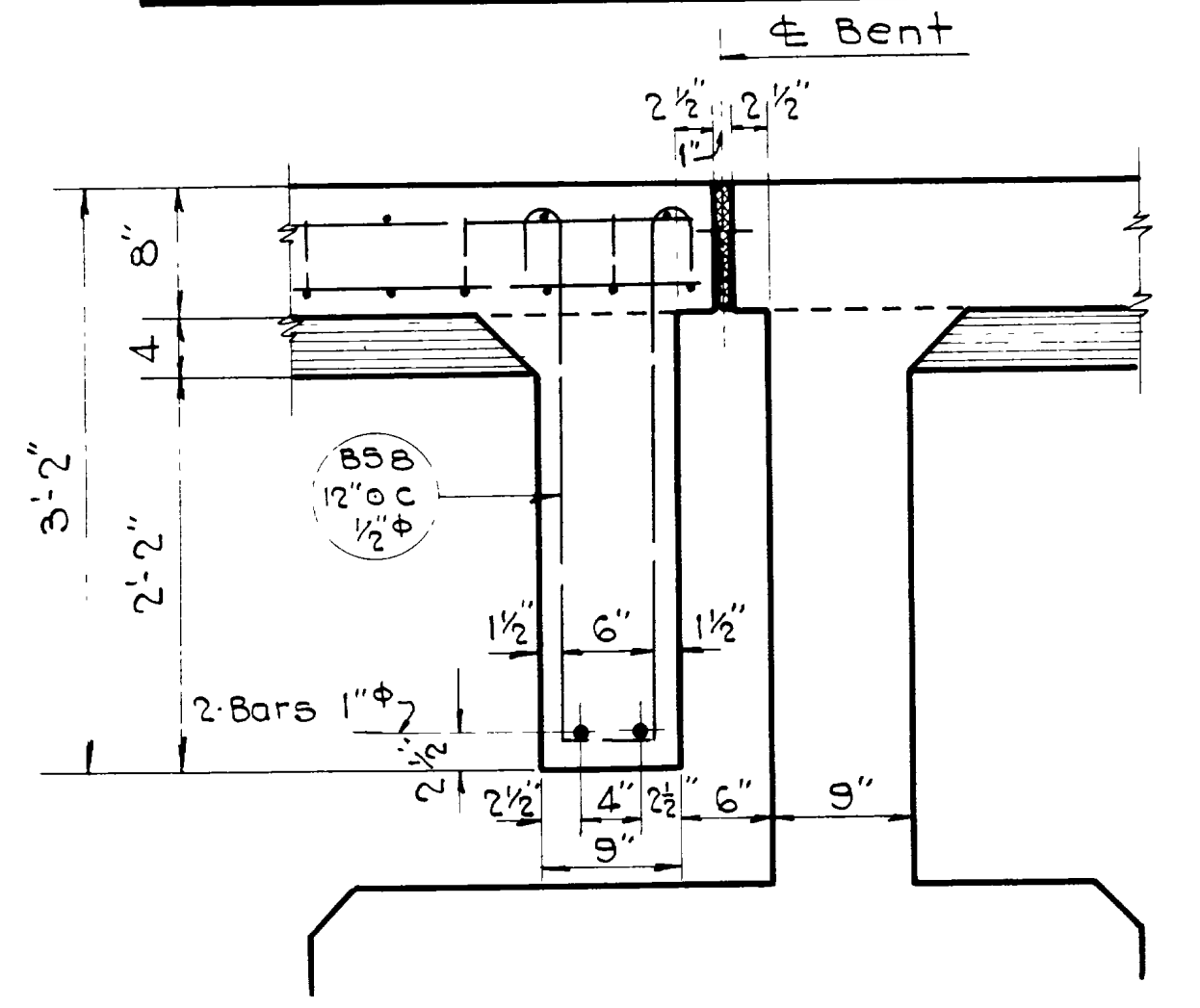
**SECTION PARALLEL TO  $\phi$  OF BENTS SHOWING STEEL IN DIAPHRAMS AT  $\phi$  OF SPANS**

Horizontal dimensions same as shown above. Drawn for Span No. 1. Span No. 2 similar. Spans No's. 4, 5 & 6 similar but opposite hand.  
1/4" = 1'-0"

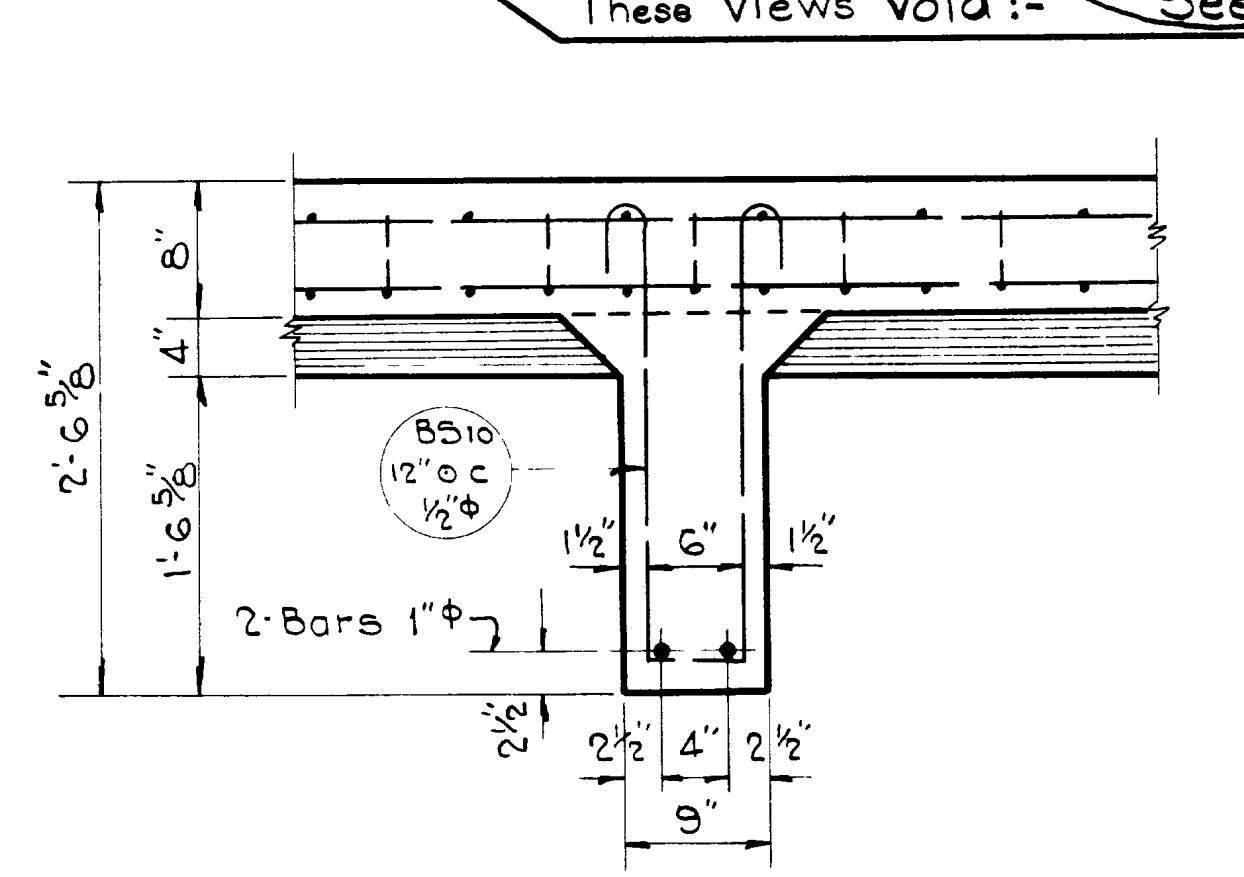


**SECTION PARALLEL TO  $\phi$  OF BENTS SHOWING STEEL IN DIAPHRAM AT  $\phi$  SPAN No. 3**

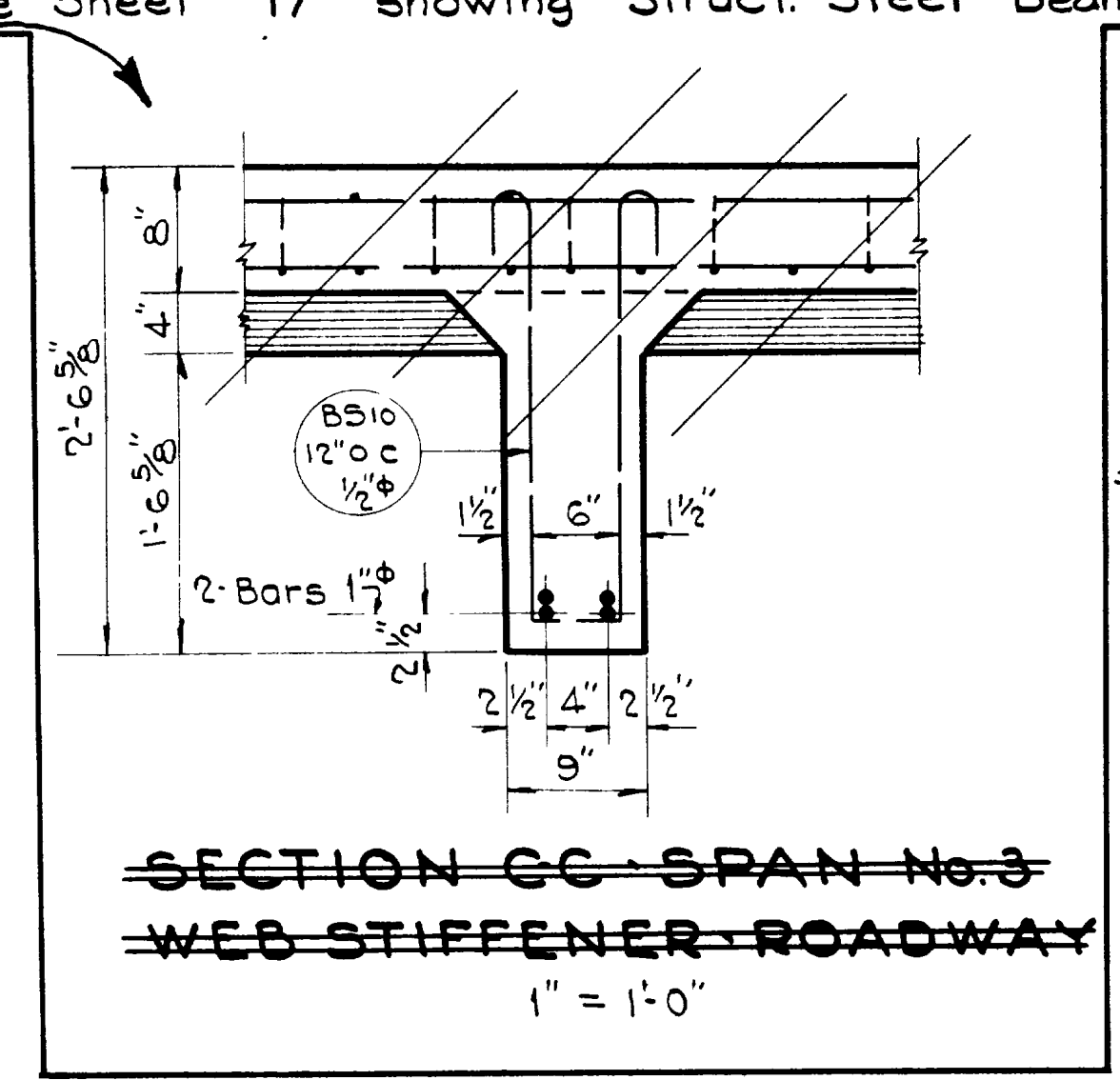
These Views Void:- See Sheet # 17 showing Struct. Steel Beams & Str. Steel Diaphr. @  $\phi$  of Span No. 3.  
1/4" = 1'-0"



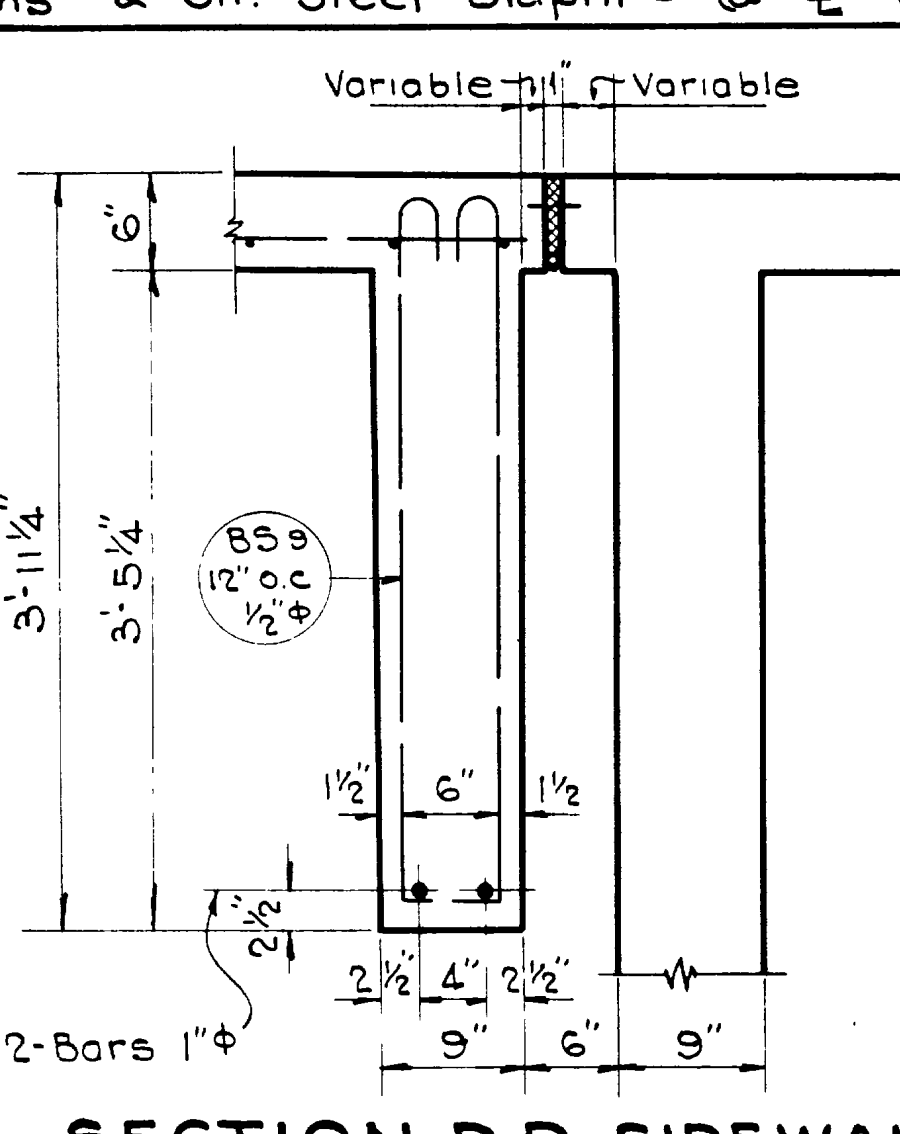
**SECTION A-A SPANS 1,2,4,5 & 6 FREE END DIAPHRAMS**  
1" = 1'-0"



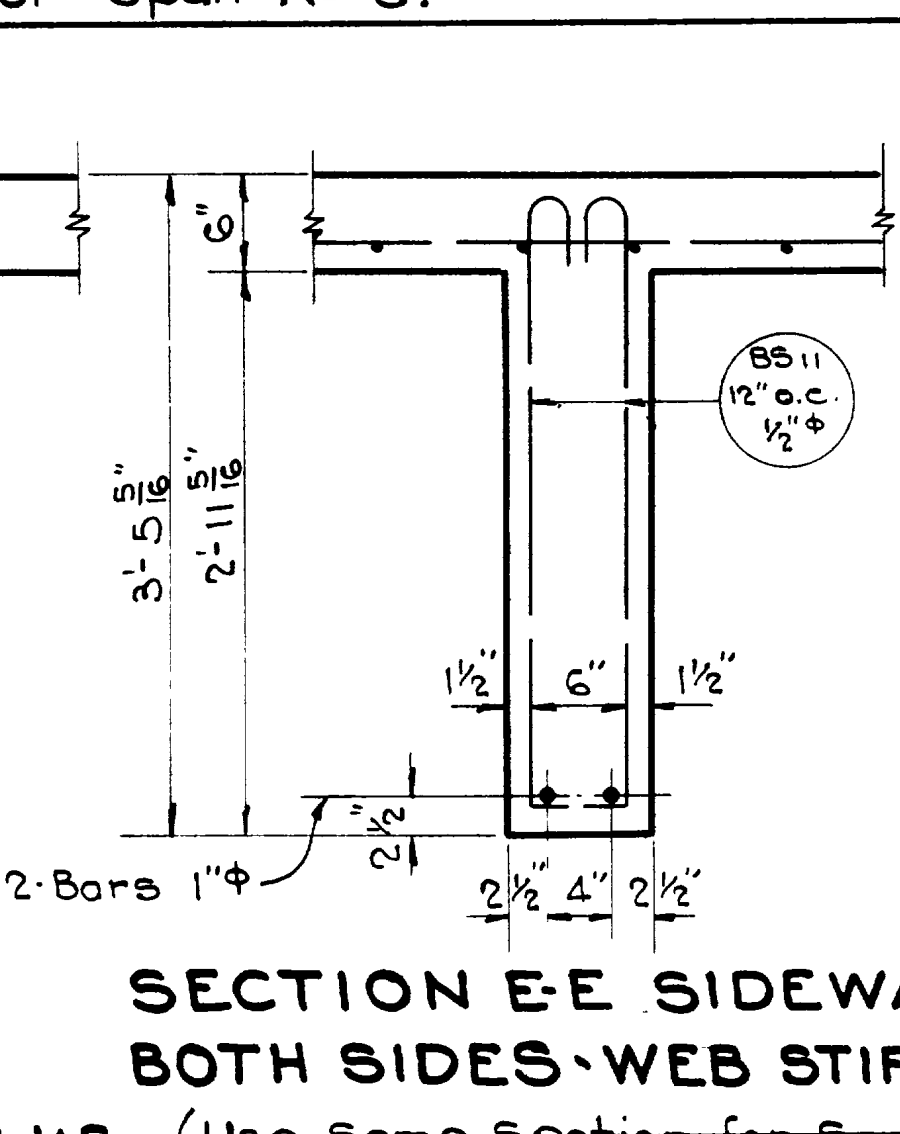
**SECTION B-B SPANS 1,2,4,5 & 6 WEB STIFFENERS ROADWAY**  
1" = 1'-0"



**SECTION C-C SPAN No. 3 WEB STIFFENER ROADWAY**  
1" = 1'-0"



**SECTION D-D SIDEWALKS BOTH SIDES FREE DIAPHRAMS**  
1" = 1'-0"



**SECTION E-E SIDEWALKS BOTH SIDES WEB STIFFENER**  
1" = 1'-0"

**FREE DIAPHRAMS AND WEB STIFFENERS**

Sheet 16 of 25 sheets  
UTAH STATE ROAD COMMISSION  
BRIDGE DEPARTMENT  
**RIVERDALE OVERHEAD OVER U.P.R.R. TRACKS**  
Sta. 121+14.47 U.G.I. 31(6)  
Riverdale Revision Weber Co.

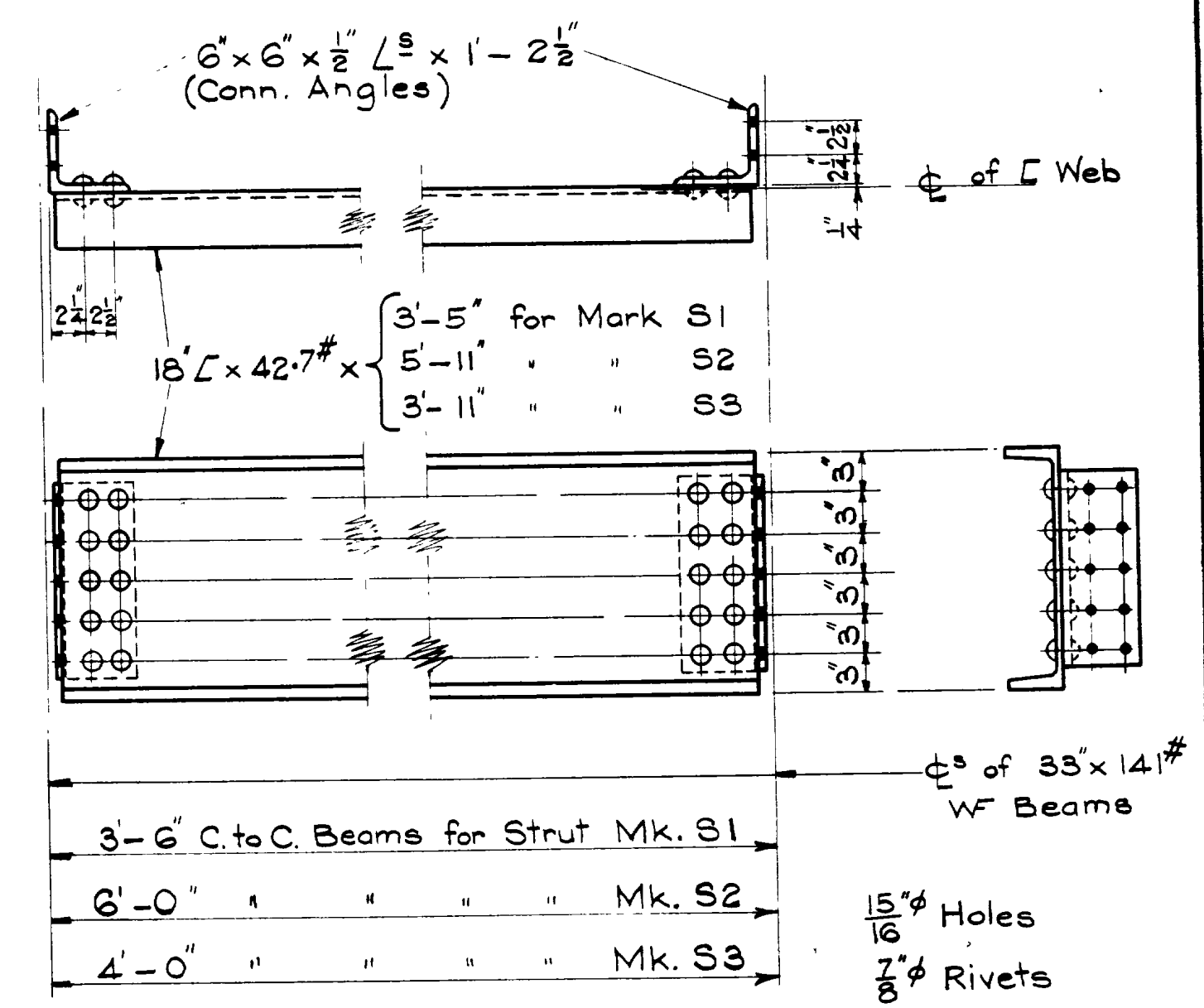
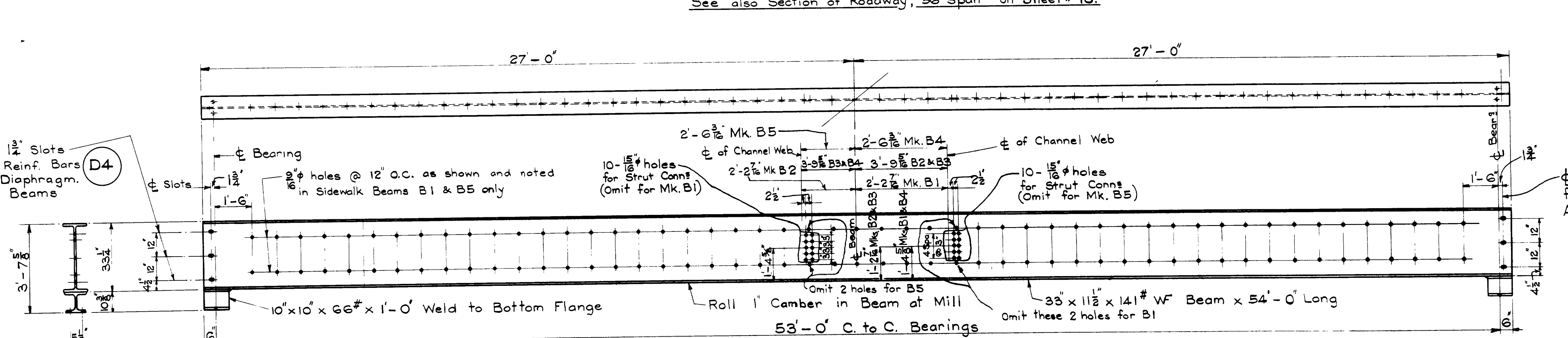
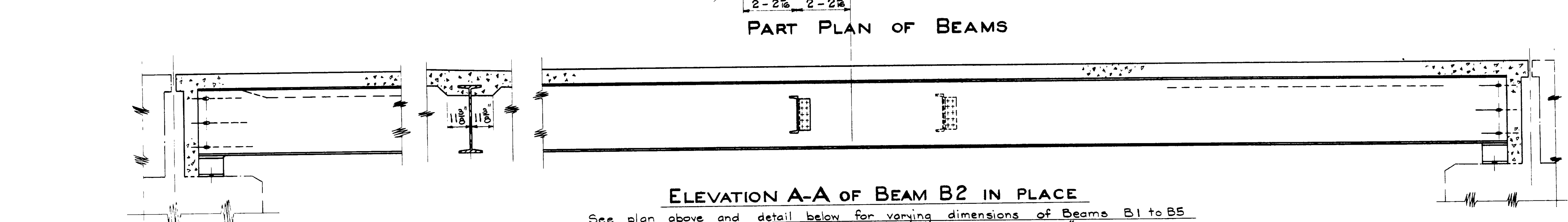
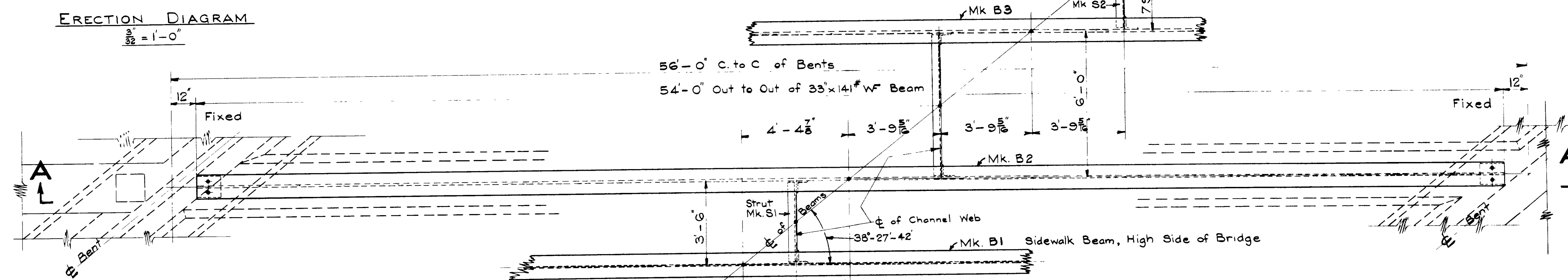
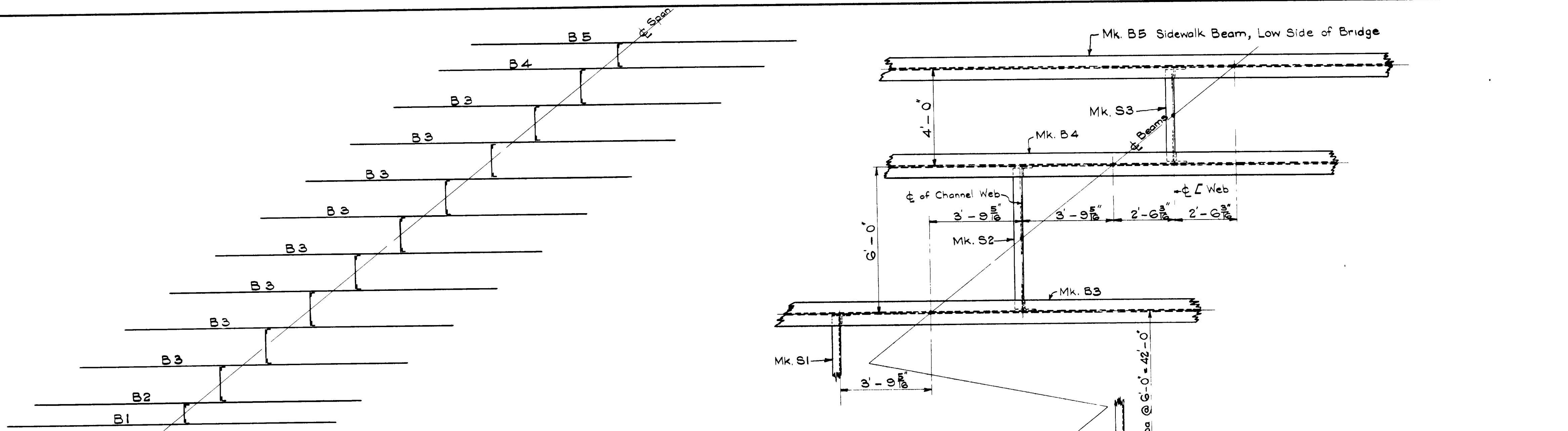
DESIGNED BY: F.M.E.  
DRAWN BY: F.M.E.  
CHECKED BY: [Signature]  
APPROVED BY: [Signature]

SCALE: As noted  
ISSUED: [Date]  
NO. 29-21-4-7 D.P. NO. D-485

REVISIONS	DATE	BY



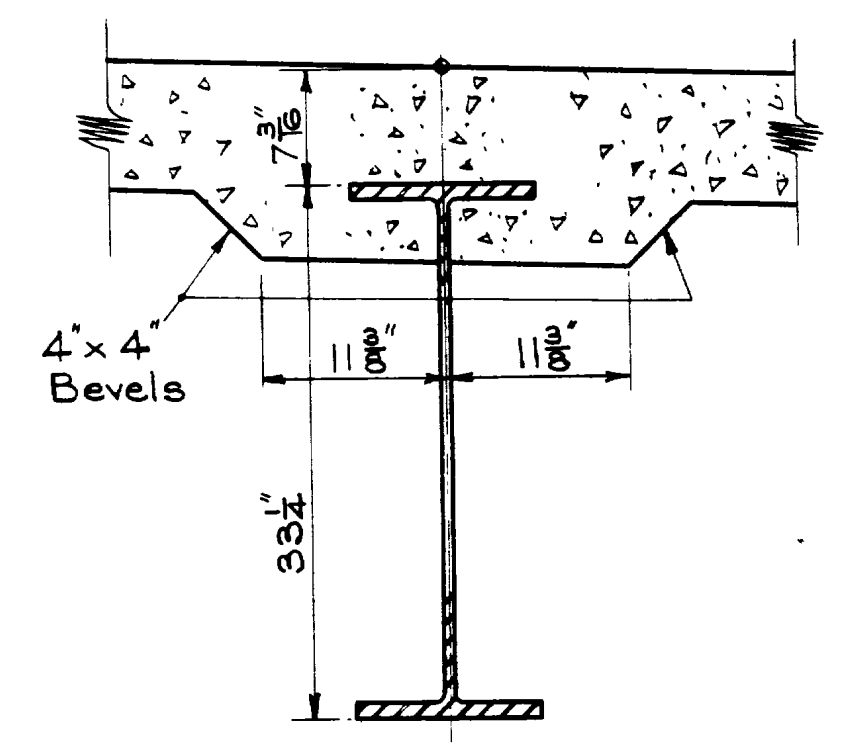
F.A.P.					
FED. ROAD DIST. NO.	STATE	U.G.I. PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
12	UTAH	31(5)			



**STRUT. STRUCT. STEEL**

Make 1 Thus as shown and noted Mk. S1 (Wt. of 1=207)  
 Make 9 Thus " " " " Mk. S2 (Wt. of 1=310)  
 Make 1 Thus " " " " Mk. S3 (Wt. of 1=225)

1" = 1'-0"



**ENLARGED SECTION**  
 showing relative position of Slab to Struct. Steel Beams  
 1" = 1'-0"

**STRUCT. STEEL BEAMS**  
 (SPAN NO. 3 ONLY).

**DETAIL OF STRUCT. STEEL BEAMS**  
 Make 8 Thus as shown and noted Mark B3  
 Make 1 each " " " " Marks B1, B2, B4 & B5.

Wt. of 1 Beam (any mark) = 7,745# ±

Scale 3/8" = 1'-0"

Sheet 17 of 25 Sheets

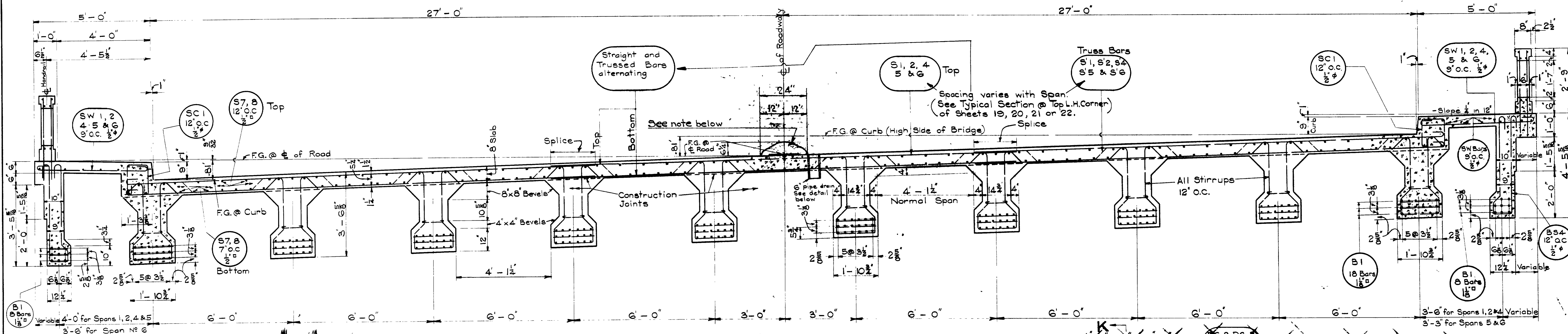
UTAH STATE ROAD COMMISSION  
 SALT LAKE CITY - UTAH  
 BRIDGE DEPARTMENT

**RIVERDALE OVERHEAD**  
 OVER U.P.R.R. TRACKS  
 Sta. 121+14.47 U.G.I. 31(5)  
 Riverdale-Revision-Weber Co.

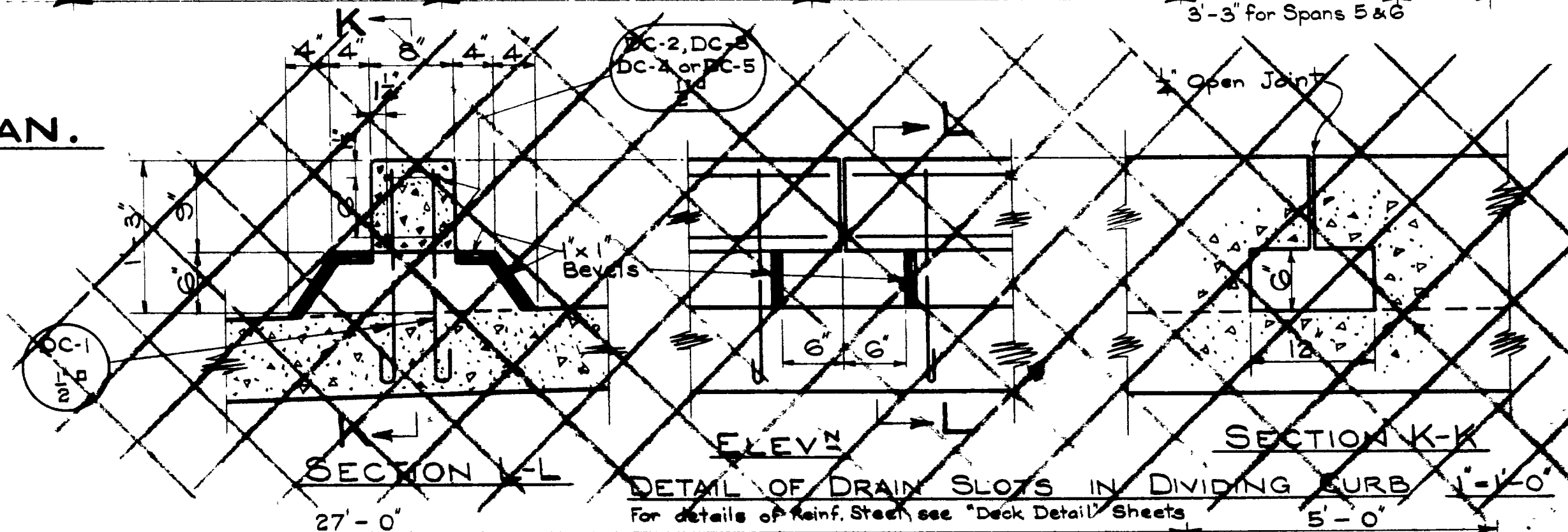
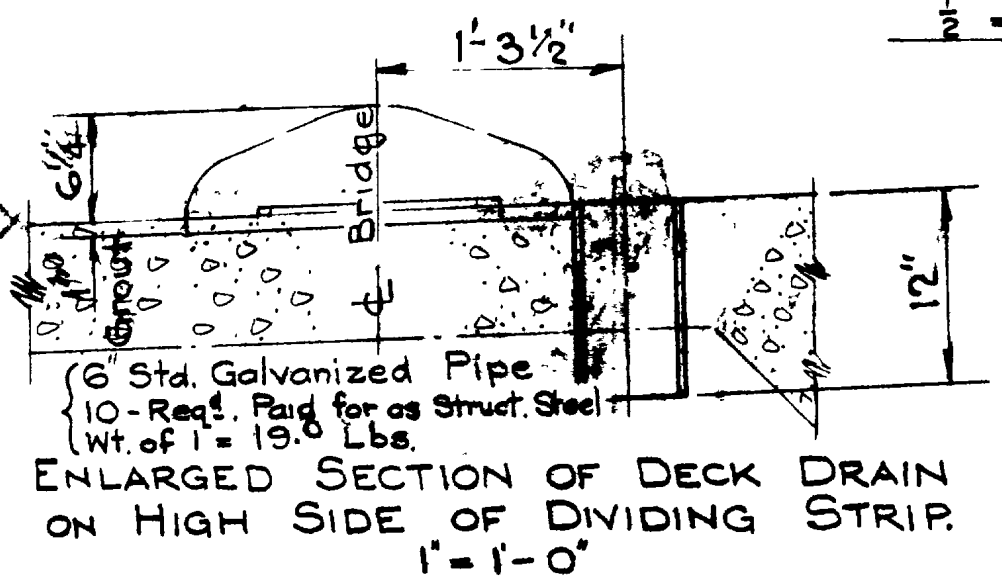
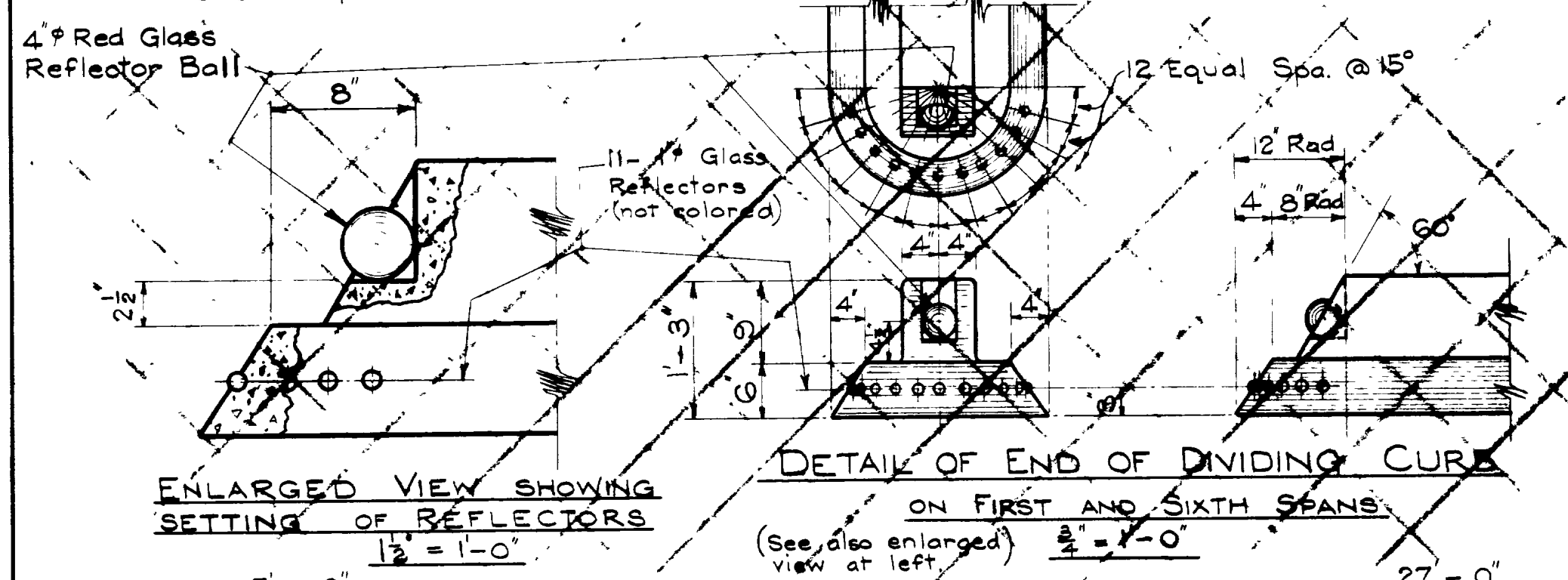
DESIGNED BY: F.M.E. SCALE: As noted  
 DRAWN BY: J.H.B. ISSUED:  
 CHECKED BY: APPROVED: CHIEF BRIDGE ENGINEER

NO. 29-21-4-7 Dwg. NO. D-485

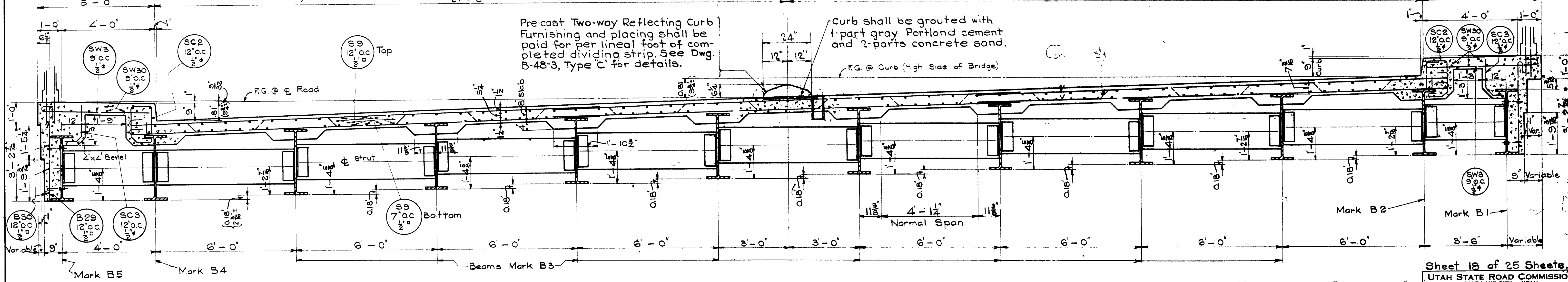
REVISIONS	DATE	BY



SECTION OF ROADWAY, 65' SPANS, @  $\phi$  SPAN.



REVISIONS	DATE	BY



SECTION OF ROADWAY, 56' SPAN - (3<sup>RD</sup> SPAN) @  $\phi$  SPAN.

All Beams 33' x 141# WF

(Structural Steel Beams)  
See details on Sheet # 17

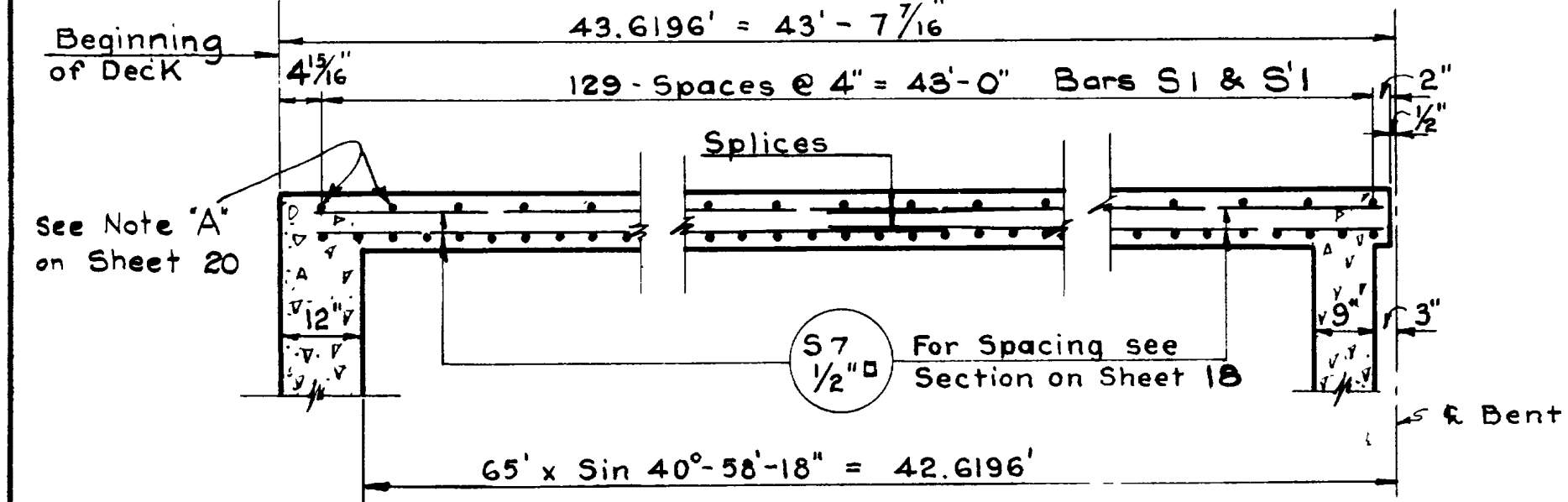
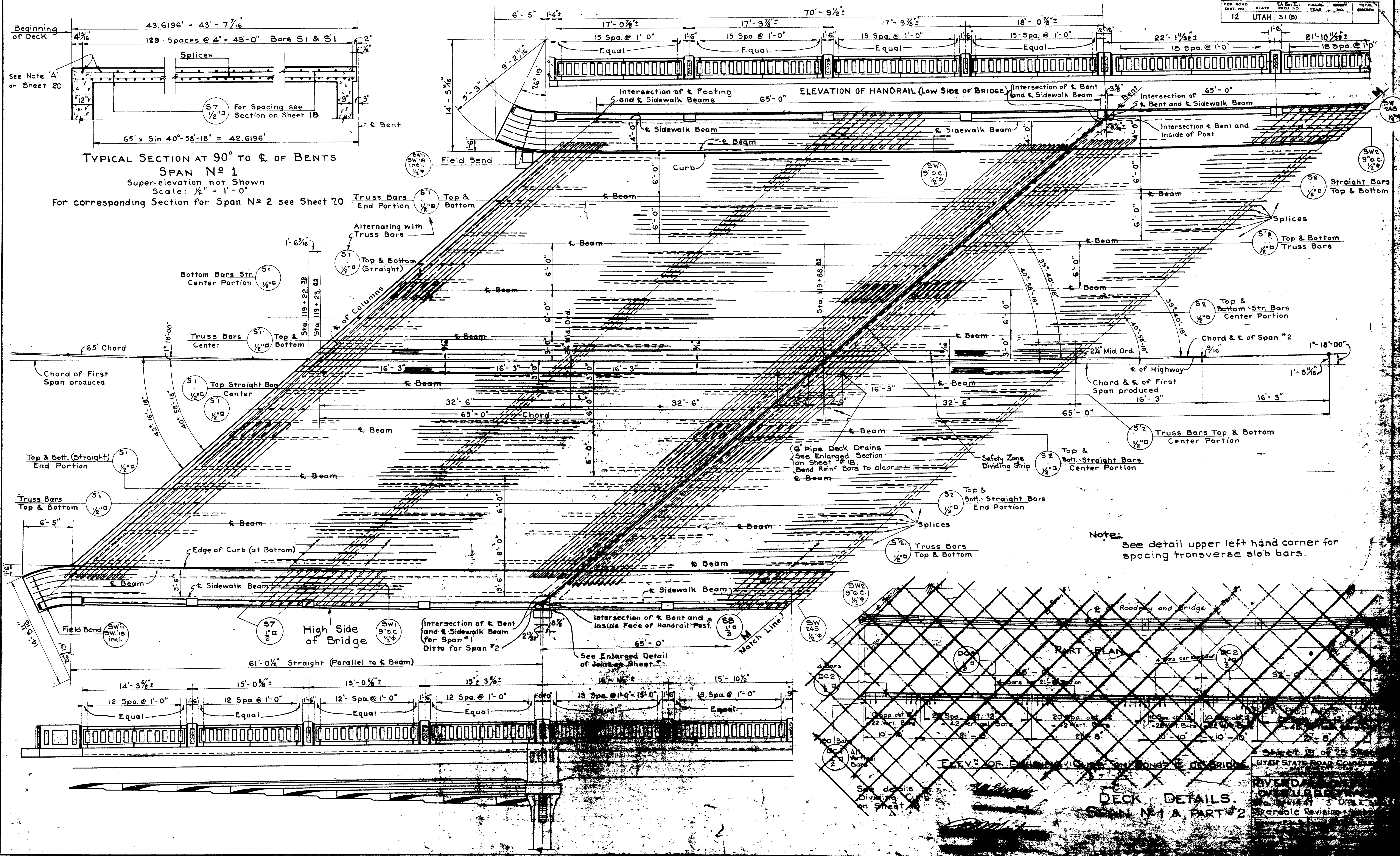
Scale  $\frac{1}{2}$ " = 1'-0"

ROADWAY SECTIONS  
(Normal to  $\phi$  of Roadway)

Sheet 18 of 25 Sheets,  
UTAH STATE ROAD COMMISSION,  
SALT LAKE CITY, UTAH  
BRIDGE DEPARTMENT  
**RIVERDALE OVERHEAD  
OVER U.P.R.R. TRACKS**  
Sta. 121+14.47 U.G.I. 31(5)  
Riverdale Revision: Weber

DESIGNED BY F.M.E. SCALE As noted  
DRAWN BY J.H.B. ISSUED  
CHECKED BY APPROVED CHIEF ENGINEER  
MAR 1 1951  
NO. 29-21-4-7 REC NO. D-485





REVISIONS	DATE	BY

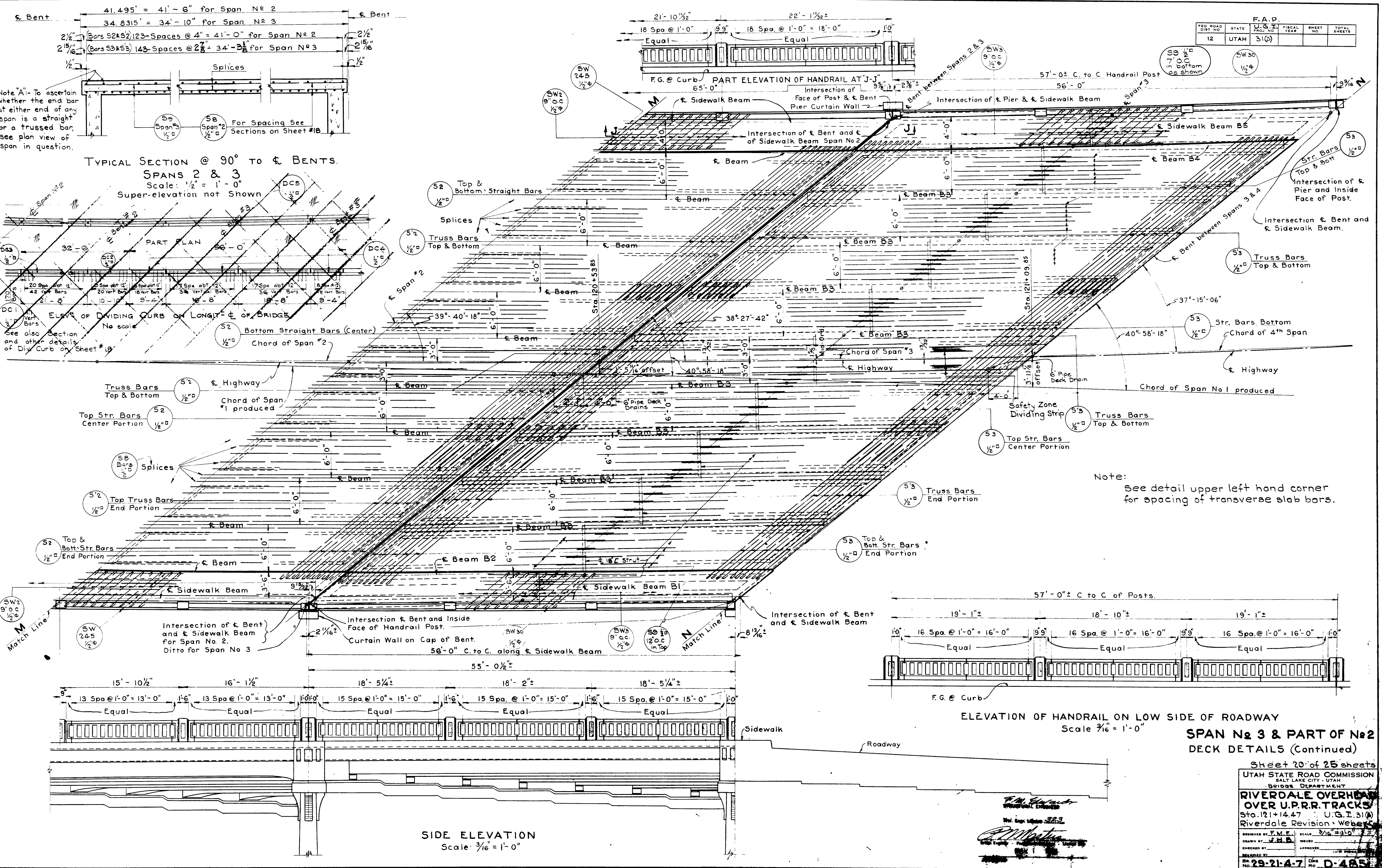
Note: See detail upper left hand corner for spacing transverse slab bars.

DECK DETAILS.  
 SPAN No 1 & PART #2

UTAH STATE ROAD COMMISSION  
 RIVERDALE  
 OVERHAULING  
 3 DAY E.S.  
 General Revision



FED. ROAD DIST. NO.	STATE	U.S. PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
12	UTAH	3(6)			



REVISIONS	DATE	BY

Note:  
See detail upper left hand corner for spacing of transverse slab bars.

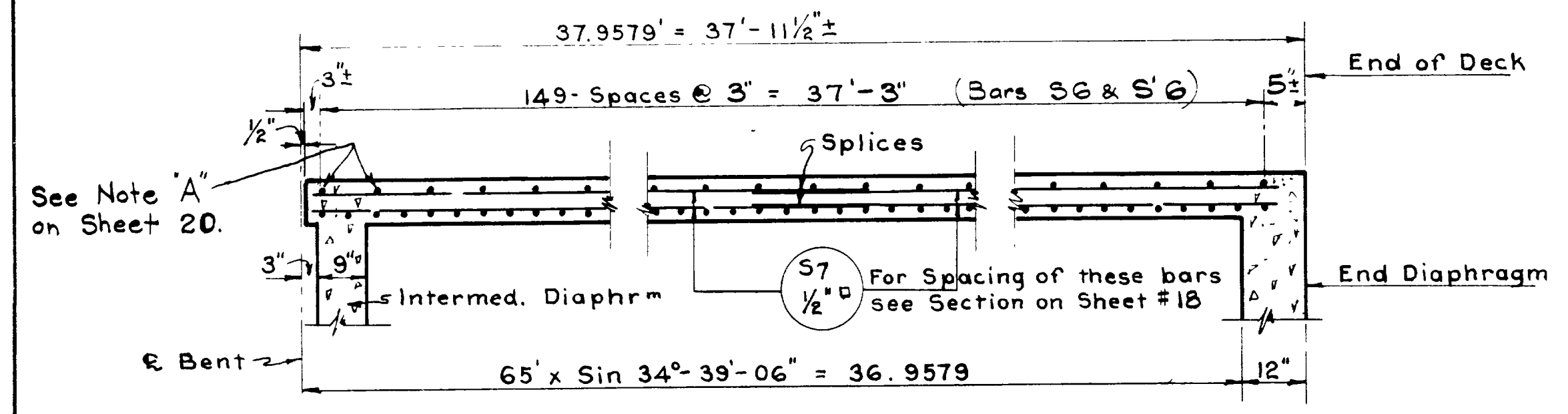
*F.M. Weber*  
Engr. License 3223  
*W. J. ...*

DESIGNED BY	SCALE
DRAWN BY	ISSUED
CHECKED BY	APPROVED
REVISIONS	DATE



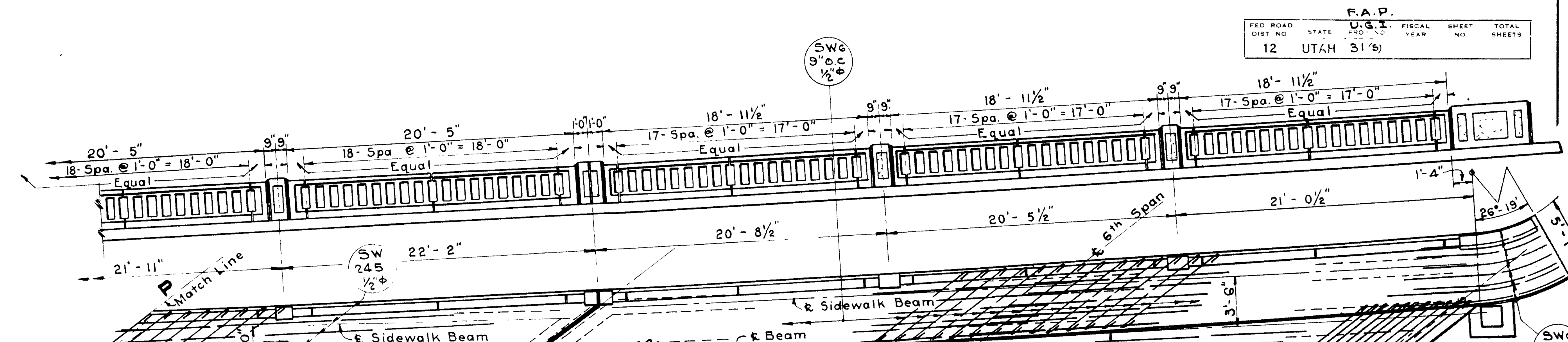




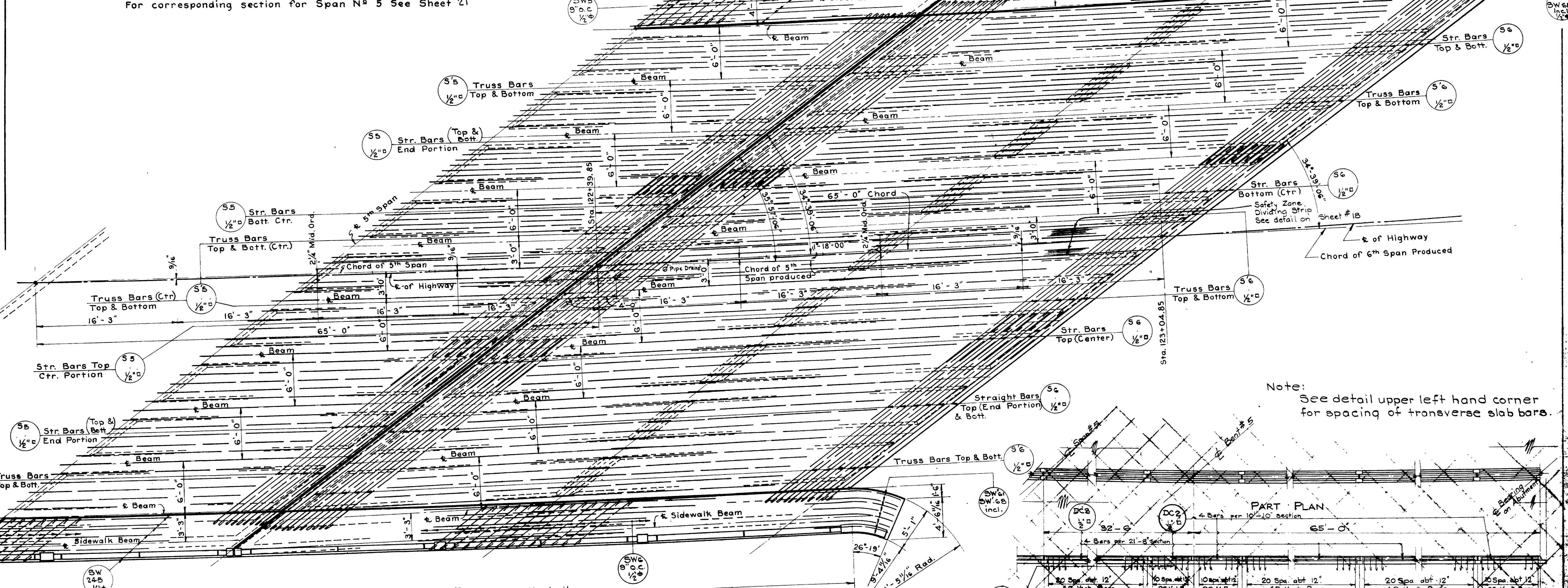


TYPICAL SECTION @ 90° TO CL OF BENTS, SPAN N<sup>o</sup> 6.  
 Super-elevation not shown  
 Scale: 1/2" = 1'-0"

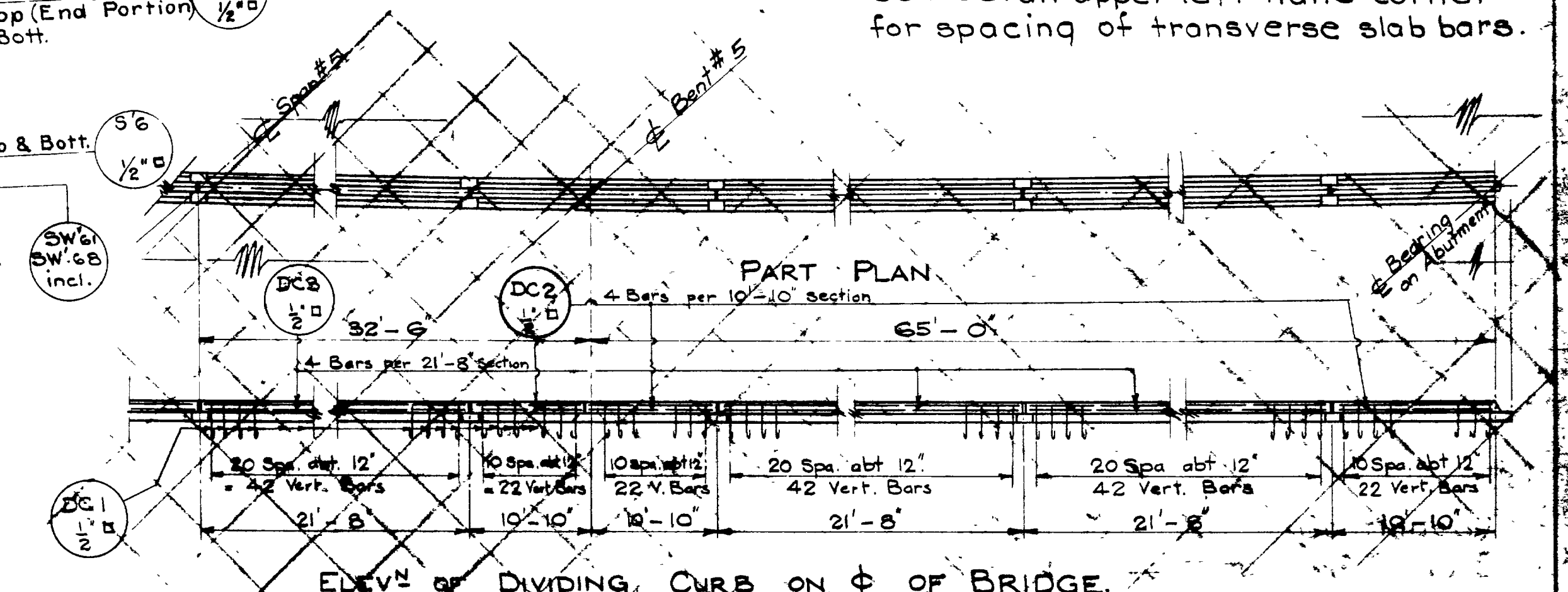
For corresponding section for Span N<sup>o</sup> 5 See Sheet 21



REVISIONS	DATE	BY



Note:  
 See detail upper left hand corner  
 for spacing of transverse slab bars.



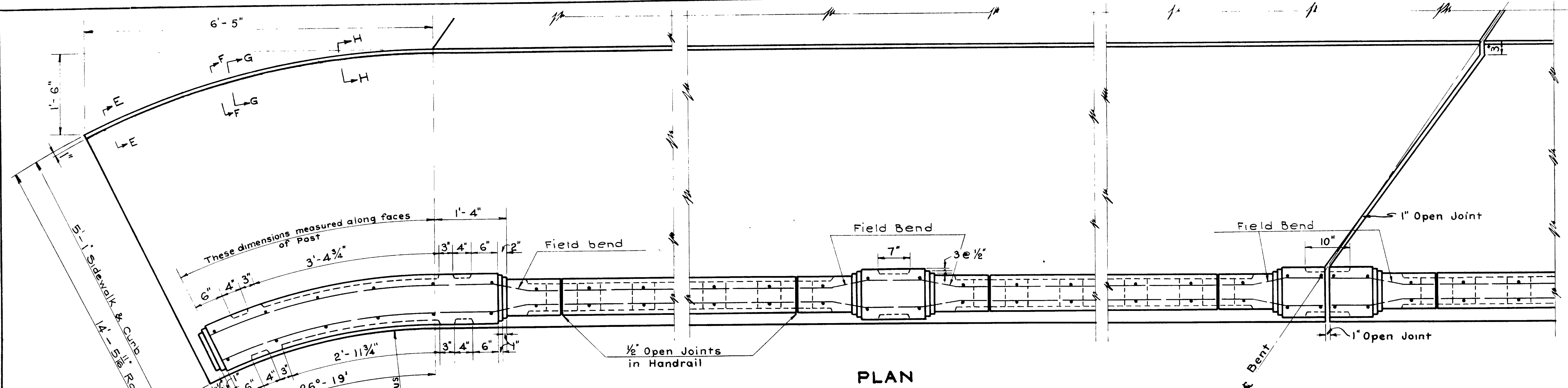
ELEV<sup>n</sup> OF DIVIDING CURB ON CL OF BRIDGE.  
 See Detail of  
 Dividing Curb  
 on Sheet # 18

DECK DETAILS  
 (continued)  
 SPAN # 6 & PART # 5

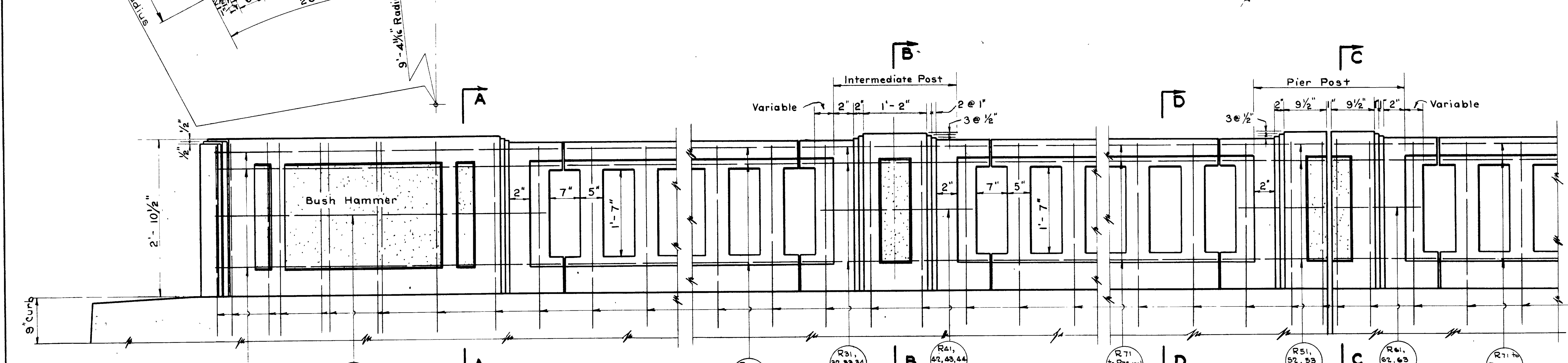
Note: Elevation of Handrail shows spacing of Sections only, and does not indicate grades or Camber.

Sheet 22 of 25 sheets  
 UTAH STATE ROAD COMMISSION  
 SALT LAKE CITY, UTAH  
 BRIDGE DEPARTMENT  
**RIVERDALE OVERHEAD**  
**OVER U.P.R.R. TRACKS**  
 Sta. 121+14.47 - U.S. 89, 310  
 Riverdale Revision: Weber  
 DESIGNED BY: E.P.E. SCALE: 1/4" = 1'-0"  
 DRAWN BY: J.M.W.  
 CHECKED BY: J.M.W.  
 APPROVED BY: J.M.W.  
 FEB 28 1957

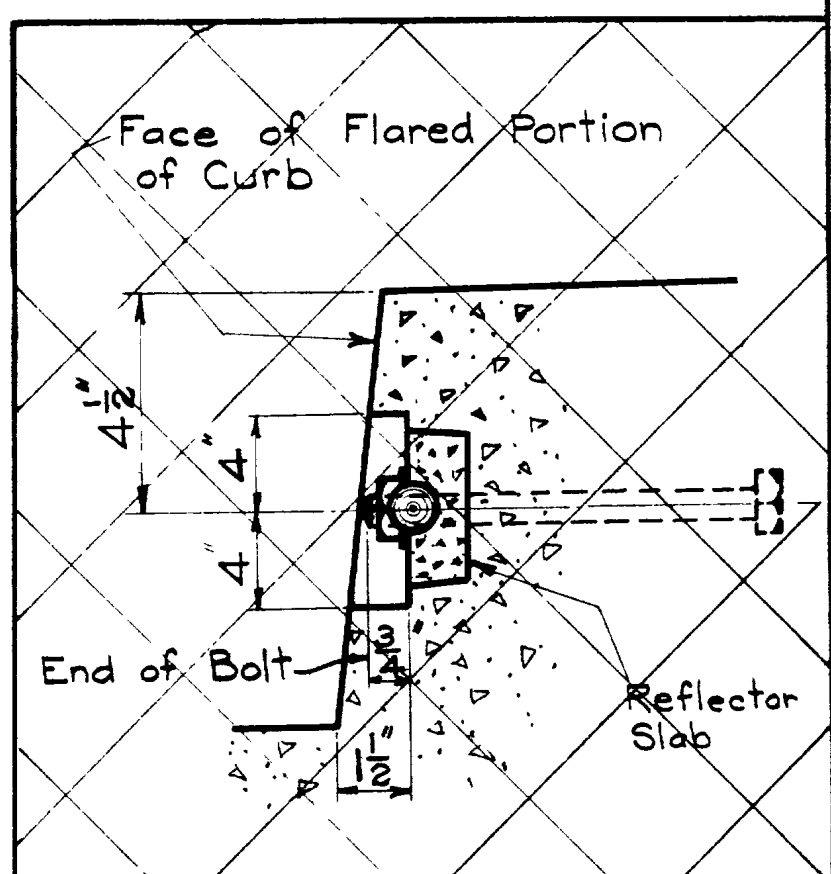




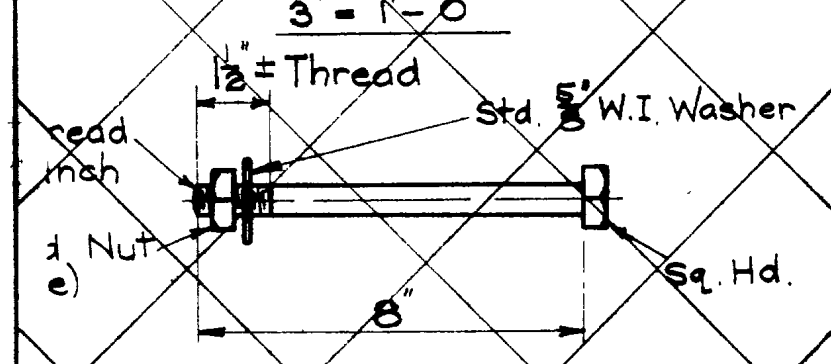
PLAN



ELEVATION



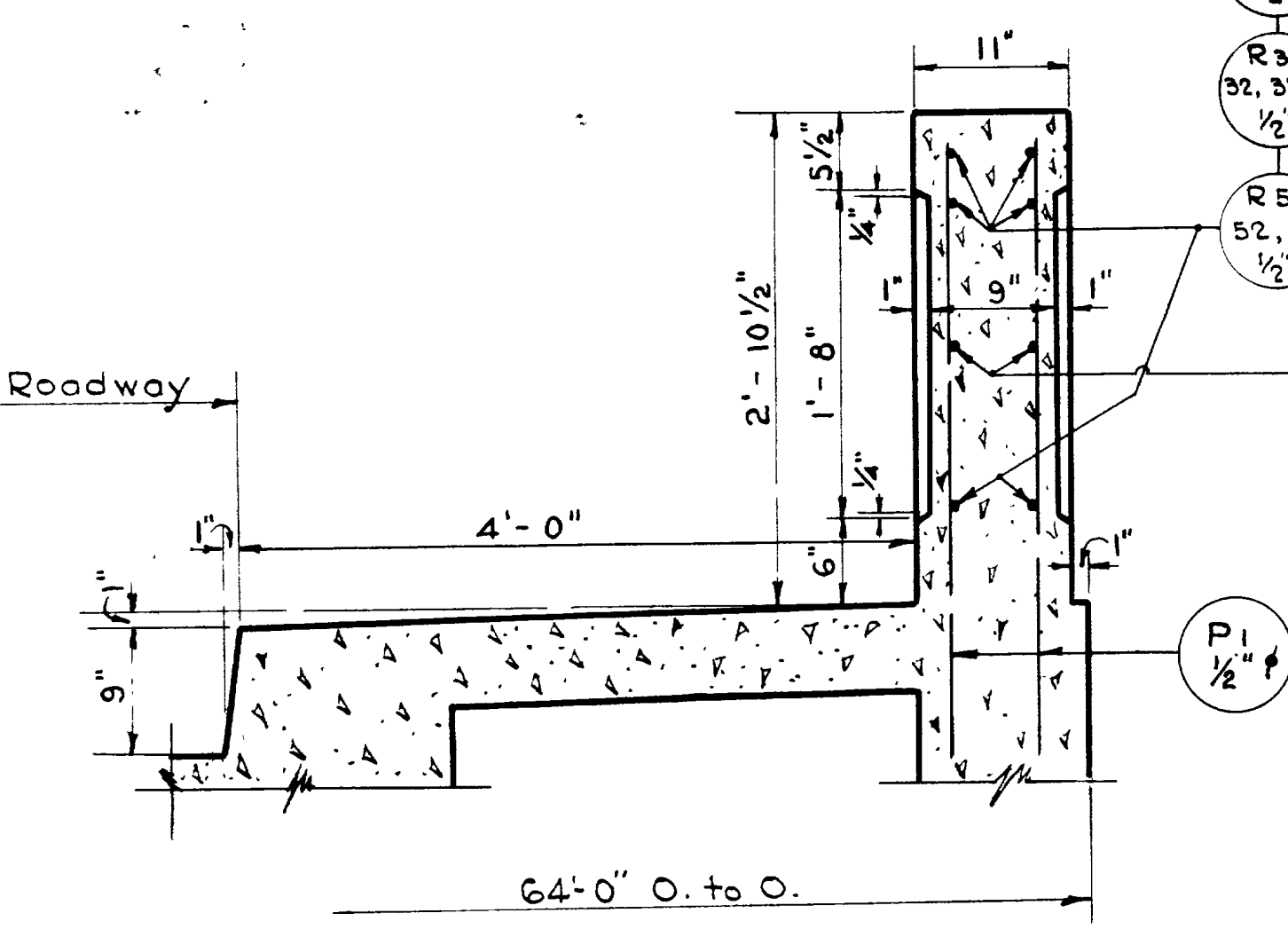
SECTIONS E-E, F-F, G-G & H-H  
 Showing setting of Reflector Slabs  
 to avoid breakage by blade of  
 snow-plow.



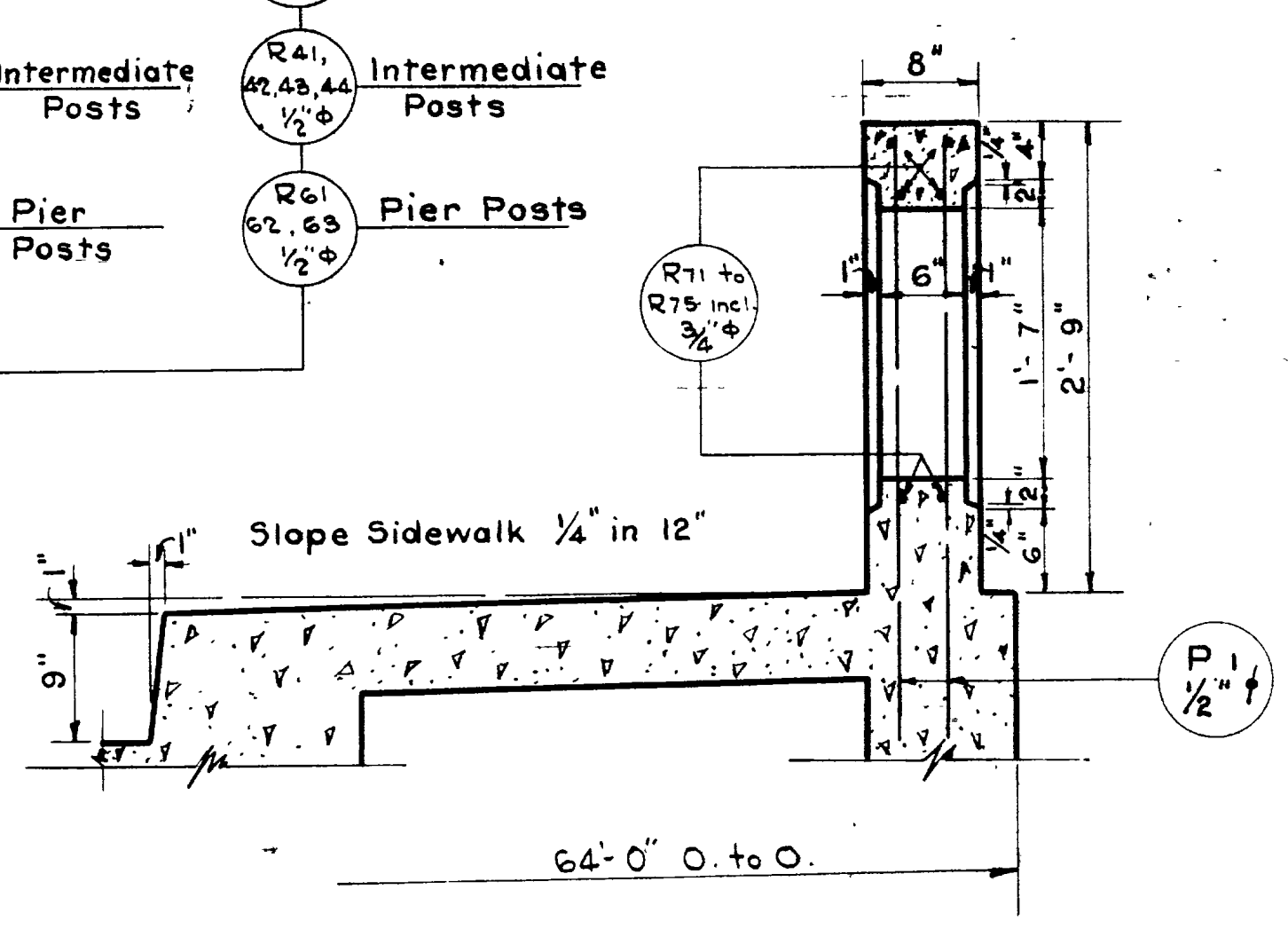
**BOLT STEEL**  
 for Reflector Slabs  
 40 Req'd Complete as shown.  
 Weight each = 1" approx  
 cluded in Struct. Steel Quantity

**CONCRETE QUANTITIES**

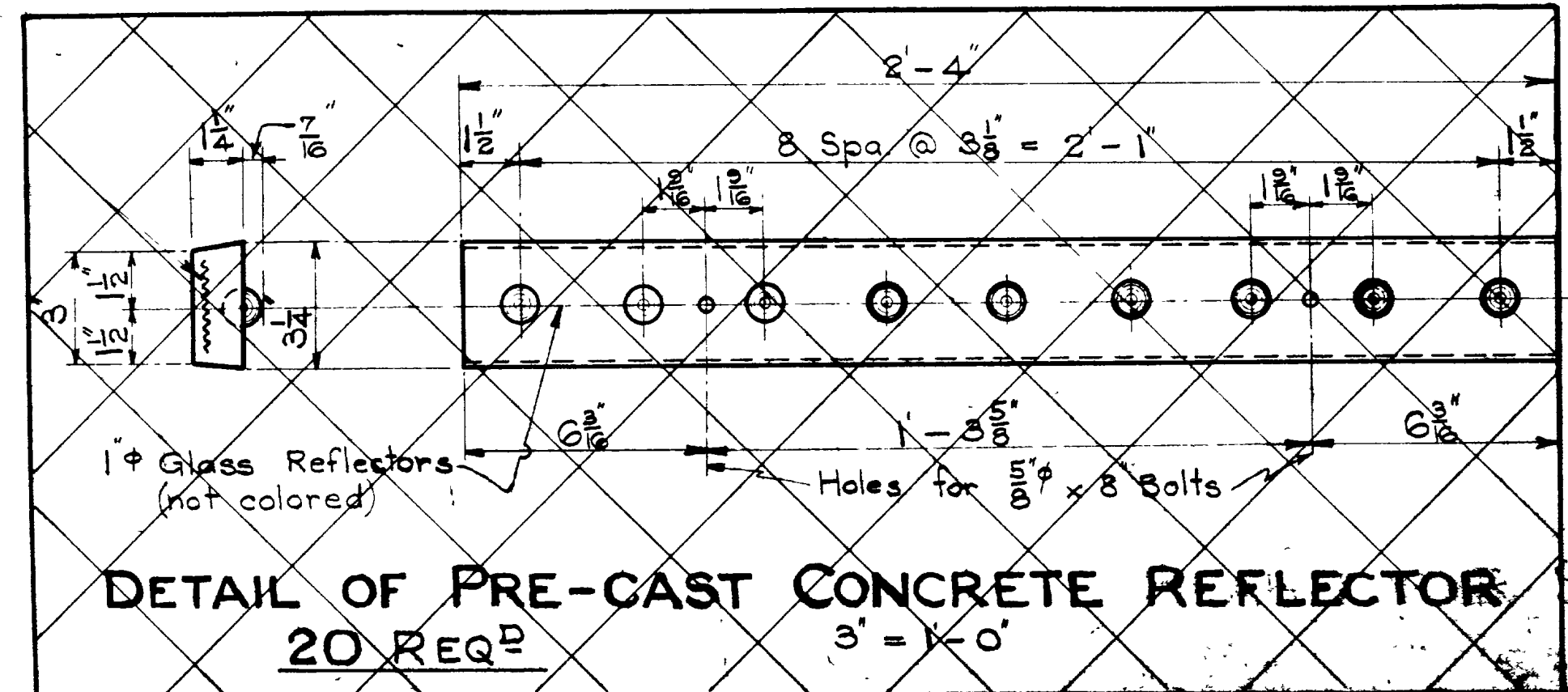
One End Post	14.30 Cu. Ft
One Pier Post	7.91 " "
One Intermediate Post	5.66 " "
Railing per Lin. Ft.	1.19 " "
Variable Portion of Railing per Lin. Ft.	1.51 " "



POST SECTIONS A-A, B-B & C-C



HANDRAIL SECTION D-D



DETAIL OF PRE-CAST CONCRETE REFLECTOR  
 20 REQ'D

**HANDRAIL DETAILS**

*F.M. Edwards*  
 STRUCTURAL ENGINEER  
 1914 ENG. LICENSE 323

*M. [Signature]*  
 APPROVED

Sheet 23 of 25 sheets  
 UTAH STATE ROAD COMMISSION  
 SALT LAKE CITY - UTAH  
 BRIDGE DEPARTMENT  
**RIVERDALE OVERHEAD  
 OVER U.P.R.R. TRACKS**  
 Stg. 121+14.47 U.G.P. 318  
 Riverdale Revision: Weber  
 DESIGNED BY E.M.E. SCALE 1"=1'-0"  
 DRAWN BY R.W.M. CHECKED BY [Signature]  
 REVISIONS BY [Signature] APPROVED [Signature]  
 No. 23-21-4-7 Date: 12-19-47 D-485

REVISIONS

NO.	DATE	BY	REASON
1	12-19-47	R.W.M.	ISSUED FOR CONSTRUCTION

REVISIONS  
DATE BY

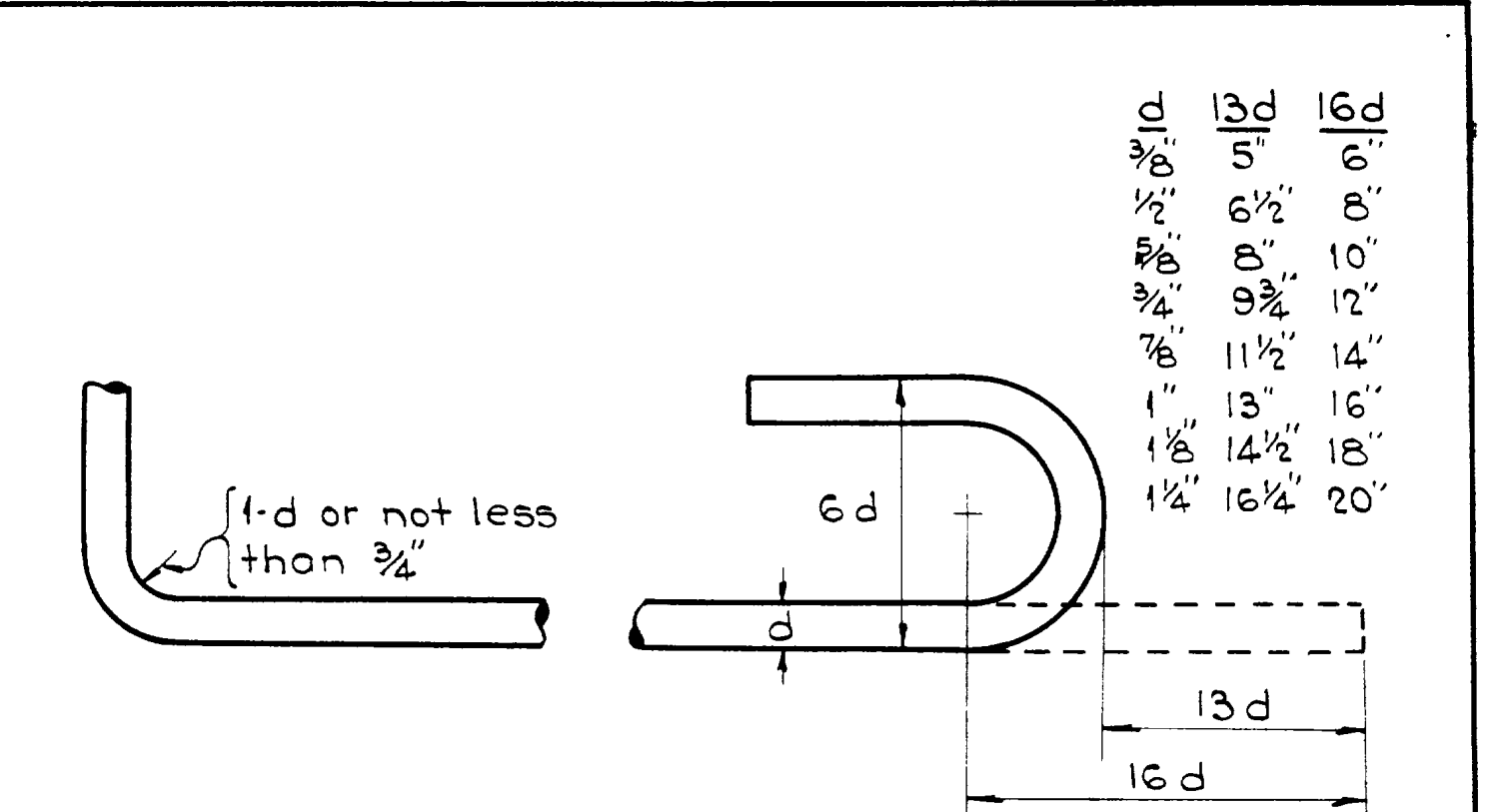
MARK	LOCATION	SIZE	LGTH	No BARS	TOTAL LENGTH	SKETCH	O.T.O.
C1	Col's. Bent #1	1/2"	36-11"	8	295'-4"		34'-2"
C2			37-1"	4	148'-4"		34'-4"
C3			37-2"	4	148'-8"		34'-5 1/2"
C4			37-4"	4	149'-4"		34'-7"
C5			37-5"	4	149'-8"		34'-8 1/2"
C6			37-7"	4	150'-4"		34'-10"
C7			37-8"	4	150'-8"		34'-11"
C8			37-10"	4	151'-4"		35'-0"
C9			37-11"	4	151'-8"		35'-2"
C10	Col's. Bent #1	1/2"	38'-0"	8	304'-0"		35'-3"
C11	Col's. Bent #2	1/2"	36'-8"	8	294'-0"		34'-0"
C12			37-0"	4	148'-0"		34'-3"
C13			37-2"	4	148'-8"		34'-4 1/2"
C14			37-3"	4	149'-0"		34'-6 1/2"
C15			37-5"	4	149'-8"		34'-8"
C16			37-7"	4	150'-4"		34'-10"
C17			37-9"	4	151'-0"		35'-0"
C18			37-11"	4	151'-8"		35'-2"
C19			38-0"	4	152'-0"		35'-3"
C20	Col's. Bent #2	1/2"	38-2"	8	305'-4"		35'-5"
C21	Col's. Bent #3	1/2"	36-7"	8	292'-8"		33'-10 1/2"
C22			36-10"	4	147'-4"		34'-1"
C23			37-0"	4	148'-0"		34'-3 1/2"
C24			37-3"	4	149'-0"		34'-6"
C25			37-5"	4	149'-8"		34'-8"
C26			37-7"	4	150'-4"		34'-10"
C27			37-9"	4	151'-0"		35'-0"
C28			38-0"	4	152'-0"		35'-2 1/2"
C29			38-2"	4	152'-8"		35'-4 1/2"
C30	Col's. Bent #3	1/2"	38-4"	8	306'-8"		35'-6 1/2"
C31	Col's. Bent #4	1/2"	36-5"	8	291'-4"		33'-8"
C32			36-8"	4	146'-8"		33'-11"
C33			36-11"	4	147'-8"		34'-2"
C34			37-2"	4	148'-8"		34'-5"
C35			37-5"	4	149'-8"		34'-8"
C36			37-8"	4	150'-8"		34'-11"
C37			37-10"	4	151'-4"		35'-1"
C38			38-1"	4	152'-4"		35'-4"
C39			38-4"	4	153'-4"		35'-7"
C40	Col's. Bent #4	1/2"	38-6"	8	308'-0"		35'-9"
C41	Col's. Bent #5	1/2"	36-3"	8	290'-0"		33'-5 1/2"
C42			36-6"	4	146'-0"		33'-9"
C43			36-10"	4	147'-4"		34'-0 1/2"
C44			37-1"	4	148'-4"		34'-2 1/2"
C45			37-4"	4	149'-4"		34'-7 1/2"
C46			37-8"	4	150'-8"		34'-11"
C47			37-11"	4	151'-8"		35'-2"
C48			38-2"	4	152'-8"		35'-5 1/2"
C49			38-6"	4	154'-0"		35'-8 1/2"
C50	Col's. Bent #5	1/2"	38-8"	8	310'-0"		35'-11 1/2"
F1	Ftgs. Bents 1 to 5	1"	9'-8"	538	5200'-8"		7'-6"
F2	" " 1	1/2"	35'-6"	24	852'-0"		
F3	" " 2	1/2"	36'-6"	24	876'-0"		
F4	" " 3	1/2"	37'-2"	24	892'-0"		
F5	" " 4	1/2"	38'-2"	24	916'-0"		
F6	" " 5	1/2"	39'-6"	24	948'-0"		
W1	Diaphragm Bents 1 to 5	1/2"	12'-9"	744	9486'-0"		11'-8"
W2	" " 1	1/2"	34'-6"	54	1863'-0"		
W3	" " 2	1/2"	35'-6"	54	1917'-0"		
W4	" " 3	1/2"	36'-2"	54	1953'-0"		
W5	" " 4	1/2"	37'-2"	54	2007'-0"		
W6	" " 5	1/2"	38'-6"	54	2079'-0"		
W7	Diaphragm Bent 1	1"	35'-8"	6	214'-0"		
W8	" " 2	1"	36'-8"	6	220'-0"		
W9	" " 3	1"	37'-4"	6	224'-0"		
W10	" " 4	1"	38'-4"	6	230'-0"		
W11	" " 5	1"	39'-8"	6	238'-0"		

MARK	LOCATION	SIZE	LGTH	No BARS	TOTAL LENGTH	SKETCH	O.T.O.
H1	Col's. Bents 1 to 5	1/2"	8'-0"	2053	16,424'-0"		
CC1	Cap. Bent 1	1"	14'-0"	80	1120'-0"		
CC2	" " 2	1"	14'-3"	80	1140'-0"		
CC3	" " 3	1"	14'-9"	80	1180'-0"		
CC4	" " 4	1"	15'-1"	80	1206'-8"		
CC5	" " 5	1"	15'-5"	80	1233'-4"		
CC1-1	Cap. Bent 1	1"	15'-6"	5	77'-6"		
CC1-2	" " 1	1"	14'-6"	5	72'-6"		
CC1-3	" " 1	1"	16'-0"	5	80'-0"		
CC1-4	" " 1	1"	15'-0"	5	75'-0"		
CC2-1	Cap. Bent 2	1"	15'-9"	5	78'-9"		
CC2-2	" " 2	1"	15'-0"	5	75'-0"		
CC2-3	" " 2	1"	16'-3"	5	81'-3"		
CC2-4	" " 2	1"	15'-6"	5	77'-6"		
CC3-1	Cap. Bent 3	1"	14'-9"	5	73'-9"		
CC3-2	" " 3	1"	13'-9"	5	68'-9"		
CC3-3	" " 3	1"	15'-3"	5	76'-3"		
CC3-4	" " 3	1"	14'-3"	5	71'-3"		
CC4-1	Cap. Bent 4	1"	14'-6"	5	72'-6"		
CC4-2	" " 4	1"	13'-6"	5	67'-6"		
CC4-3	" " 4	1"	15'-0"	5	75'-0"		
CC4-4	" " 4	1"	14'-0"	5	70'-0"		
CC5-1	Cap. Bent 5	1"	15'-0"	5	75'-0"		
CC5-2	" " 5	1"	13'-8"	5	68'-4"		
CC5-3	" " 5	1"	15'-6"	5	77'-6"		
CC5-4	" " 5	1"	14'-2"	5	70'-10"		
CC6	Cap. Bents 1 to 5	1/2"	5'-3"	998	5239'-6"		4'-2"
D1	Cap. Bents 1 to 5	1/2"	6'-4"	854	5408'-8"		5'-3"
D2	Cap Ends Bents 1 to 5	1/2"	6'-0"	214	1284'-0"		
D2-1	" " 1 to 5	1/2"	3'-9"	140	525'-0"		
D3	Diaphragm Bent 1	1/2"	29'-6"	24	708'-0"		
D4	" " Bents 2 & 3	1/2"	34'-8"	36	1248'-0"		
D4-1	" " 2 & 3	1/2"	3'-4"	12	40'-0"		
D4-2	" " 2 & 3	1/2"	2'-6"	12	36'-6"		
D5	" " 4	1/2"	36'-6"	18	657'-0"		
D6	" " 5	1/2"	33'-9"	18	607'-6"		
A1	Abut. Salt Lake End	1/2"	34'-7"	24	830'-0"		
A2	" " "	1/2"	12'-4"	82	1011'-4"		
A3	" " "	1/2"	14'-2"	13	184'-2"		
A4	" " "	1/2"	12'-2"	5	60'-10"		
A5	" " "	1/2"	13'-2"	6	79'-0"		
A6	" " "	1/2"	15'-0"	6	90'-0"		
A7	" " "	1/2"	11'-0"	48	528'-0"		
A8	Abut. Salt Lake End	1/2"	4'-7"	200	916'-8"		3'-6"
A9	Abut. Wings S.L. End	1/2"	13'-11"	22	306'-2"		
A10	" " "	1/2"	11'-0"	12	132'-0"		
A11	" " "	1/2"	8'-6"	12	102'-0"		
A12	" " "	1/2"	7'-0"	12	84'-0"		

MARK	LOCATION	SIZE	LGTH	No. BARS	TOTAL LENGTH	SKETCH	O.T.O.
A'1	Abut. Ogden End	1/2"	39'-3"	24	942'-0"		
A'2	" " "	1/2"	12'-3"	97	1188'-3"		
A'3	" " "	1/2"	14'-3"	7	99'-9"	ditto but A=6'-3"	
A'4	" " "	1/2"	12'-3"	2	24'-6"	" " A=5'-3"	
A'5	" " "	1/2"	13'-7"	8	108'-8"	" " A=5'-11"	
A'6	" " "	1/2"	14'-6"	6	87'-0"		
A'7	" " "	1/2"	16'-0"	6	96'-0"		
A'8	" " "	1/2"	17'-6"	48	600'-0"		
A'9	Abut. Ogden End	1/2"	4'-7"	228	1045'-0"		3'-6"
A'10	Abut. Wings Ogden End	1/2"	13'-7"	24	326'-0"		
A'11	" " "	1/2"	7'-0"	12	84'-0"	Field bend	
A'12	" " "	1/2"	8'-0"	14	112'-0"	Field bend	
A'13	" " "	1/2"	11'-6"	14	161'-0"	Field bend	
B1	Beams Spans 1, 2, 4, 5 & 6	1/2"	40'-0"	980	39,200'-0"		
B2	Beams Spans 1, 2, 4, 5 & 6	1/2"	40'-0"	220	8,800'-0"		
B2	Int. Precast Beams	1/2"	13'-4"	240	3200'-0"		12'-11"
B3	" " "	1/2"	14'-6"	100	1450'-6"		
B4	" " "	1/2"	10'-6"	100	1066'-6"		
B5	Internal Beams Spans 1, 2, 4, 5 & 6	1/2"	2'-0"	2500	5000'-0"		0'-11"
B6	" " "	1/2"	1'-6"	240	360'-0"		
B7	" " "	1/2"	2'-6"	100	260'-6"		
B8	" " "	1/2"	0'-6"	20	120'-0"		
B9	Beam Outside (High Side)	1/2"	6'-7"	12	79'-6"		5'-6"
B10	" " "	1/2"	12'-0"	16	202'-0"		11'-9"
B11	Outside Beams (65 Spans) (Precast)	1/2"	0'-10"	240	200'-0"		
B12	" " "	1/2"	14'-6"	16	224'-6"		12'-9"
B13	Beam Appr. Span	1/2"	18'-11"	240	4540'-0"		17'-8"
B14	" " "	1/2"	20'-11"	240	4820'-0"		
B15	" " "	1/2"	16'-8"	240	4000'-0"		
B17	" " Spans 1, 2, 4, 5 & 6	1/2"	1'-9"	1200	2100'-0"		
B18	" " " " " " " "	1/2"	25'-6"	200	5100'-0"		
B19-1	Beam Diaphragm Span 1	1"	35'-5"	6	212'-6"		33'-3"
B19-2	" " " 2	1"	36'-3"	6	217'-6"		34'-11"
B19-3	" " " 4	1"	38'-11"	6	228'-6"		35'-11"
B19-5	" " " 5	1"	39'-0"	6	234'-0"		36'-10"
B19-6	" " " 6	1"	39'-10"	6	239'-0"		37'-8"
B19-8	" " " 8	1"	3'-4"	18	168'-6"		
B20	Beam Spans 2, 4 & 5	1/2"	17'-10"	360	6420'-0"		16'-7"
B21	" " "	1/2"	20'-3"	360	7290'-0"		
B22	" " "	1/2"	16'-10"	360	6060'-0"		
B23	Outside Beam (Appr.) (Low & High Side)	1/2"	18'-11"	48	908'-0"		17'-8"
B24	ditto	1/2"	20'-0"	16	320'-0"		
B25	Outside Beams (Appr. Spans 2, 4 & 5)	1/2"	1'-10"	80	146'-8"		
B26	ditto	1/2"	6'-0"	40	240'-0"		
B27	Outside Beams (Spans 2, 4 & 5)	1/2"	17'-11"	72	1290'-0"		16'-8"
B28	ditto	1/2"	19'-0"	24	456'-0"		
B29	Outside Beams (Span #2 only)	1/2"	4'-6"	108	486'-0"		3'-11 1/2"
B30	" " "	1/2"	28'-6"	16	456'-0"		Splice with each other
B31	Int. Precast Beams	1/2"	9'-4"	120	1120'-0"		

MARK	LOCATION	SIZE	LGTH	No. BARS	TOTAL LENGTH	SKETCH	O.T.O.
B32	Int. Precast Beams	1/2"	9'-6"	140	1330'-0"		
B33	Int. Precast Beams	1/2"	9'-6"	200	1960'-0"		
B34	Outside Beams (65 Spans) Low & High Sides	1/2"	10'-2"	618	6283'-0"		
B35	Beams Spans 1, 2, 4, 5 & 6	1/2"	9'-8"	600	5850'-0"		
B36	Beams Spans 1, 2, 4, 5 & 6	1/2"	10'-3"	1400	14,350'-0"		
B37	Beams Spans 1, 2, 4, 5 & 6	1/2"	10'-3"	1100	11,275'-0"		

\* See note on Sheet 2B



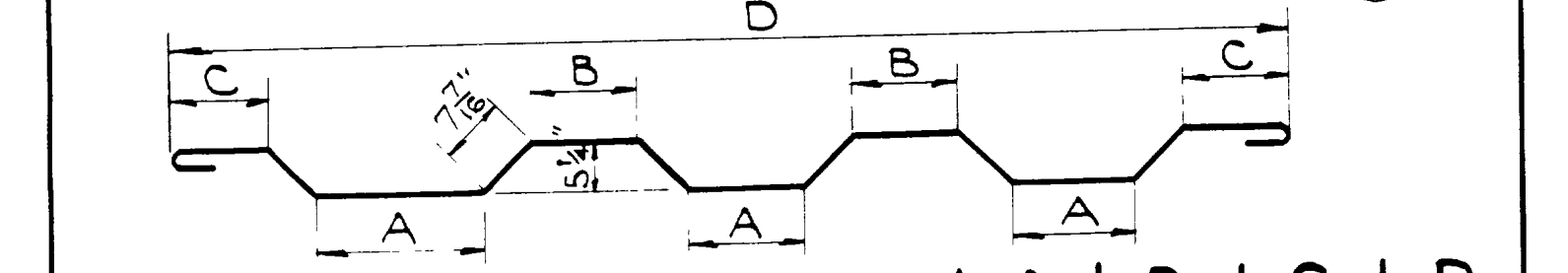
ROD NOTES

When hooks are called for the lengths given in the Total Length column include allowance for hooks as shown in above detail. Lengths given in sketches are center to center of bend points. Bar diagrams are not drawn to scale and those bars not detailed are either straight or field bent.

Sheet 24 of 25 sheets  
 UTAH STATE ROAD COMMISSION  
 SALT LAKE CITY, UTAH  
 BRIDGE DEPARTMENT  
**RIVERDALE OVERHEAD OVER U.P.R.R. TRACKS**  
 Sta. 121+14.47 U.G.I. 31(9)  
 Riverdale Revision - Weber Co.  
 DESIGNED BY: F.M.F. SCALE: None  
 DRAWN BY: F.M.F. ISSUED:  
 CHECKED BY: APPROVED: CHIEF ENGINEER  
 EXAMINED BY:  
 NO. 29-21-4-7 3RD NO. D-485



MARK	LOCATION	SIZE	LGTH.	No BARS	TOTAL LENGTH	SKETCH	O.to.O.
B5a	Beams-Free Ends (Spans 1, 2, 3 & 4)	1/2"	7'-6"	315	2362'-6"		
B5b	Beam Web Stiff. (Spans 1, 2, 3 & 4)	1/2"	6'-4"	315	1995'-0"		
B5c	Beams-Free Ends (Spans 1, 2, 3 & 4)	1/2"	9'-2"	40	366'-8"		
B5d	Beam Web Stiff. (Spans 1, 2, 3 & 4)	1/2"	8'-2"	40	326'-8"		



				A	B	C	D
S1	Slab Span 1	1/2"	33'-0"	195	6435'-0"	3'-9 3/4"	4'-5 1/8"
S2	" " 2	1/2"	33'-11"	186	6308'-6"	3'-11"	4'-7 1/2"
S3	" " 3	1/2"	34'-9"	190	6602'-6"	4'-0 1/4"	4'-9"
S4	" " 4	1/2"	35'-11"	207	7434'-9"	4'-1 1/8"	4'-10 3/8"
S5	" " 5	1/2"	36'-9"	228	8379'-0"	4'-3 3/8"	5'-1"
S6	" " 6	1/2"	37'-9"	225	8493'-3"	4'-4 3/8"	5'-3 3/8"

SIDEWALK							
SW11	Low Side Span 1	1/2"	39'-11"	2	78'-2"		
SW12	" " " "	1/2"	39'-2"		78'-4"		
SW13	" " " "	1/2"	39'-3"		78'-6"		
SW14	" " " "	1/2"	38'-10"		77'-8"		
SW15	" " " "	1/2"	38'-8"		77'-4"		
SW16	" " " "	1/2"	38'-4"		76'-8"		
SW17	" " " "	1/2"	38'-2"		76'-4"		
SW18	Low Side Span 1	1/2"	38'-0"		76'-0"		
SW11	High Side Span 1	1/2"	33'-6"		67'-0"		
SW12	" " " "	1/2"	33'-7"		67'-2"		
SW13	" " " "	1/2"	33'-10"		67'-8"		
SW14	" " " "	1/2"	34'-6"		69'-0"		
SW15	" " " "	1/2"	34'-11"		69'-10"		
SW16	" " " "	1/2"	35'-6"		71'-0"		
SW17	" " " "	1/2"	35'-11"		71'-10"		
SW18	High Side Span 1	1/2"	36'-6"	2	73'-0"		
SW245	Spans 2, 4 & 5	1/2"	33'-4"	96	3200'-0"		
SW30	Both Sides Span 3	1/2"	28'-10"	32	922'-8"		
SW61	Low Side Span 6	1/2"	34'-2"	2	68'-4"		
SW62	" " " "	1/2"	34'-4"		68'-8"		
SW63	" " " "	1/2"	34'-8"		69'-4"		
SW64	" " " "	1/2"	34'-8"		69'-0"		
SW65	" " " "	1/2"	35'-11"		71'-10"		
SW66	" " " "	1/2"	36'-7"		73'-2"		
SW67	" " " "	1/2"	37'-1"		74'-2"		
SW68	Low Side Span 6	1/2"	37'-9"		75'-6"		
SW61	High Side Span 6	1/2"	37'-2"		74'-4"		
SW62	" " " "	1/2"	37'-5"		74'-10"		
SW63	" " " "	1/2"	37'-8"		75'-4"		
SW64	" " " "	1/2"	38'-1"		76'-2"		
SW65	High Side Span 6	1/2"	38'-4"	2	76'-8"		

MARK	LOCATION	SIZE	LGTH.	No BARS	TOTAL LENGTH	SKETCH	O.to.O.
SIDEWALK							
SW66	High Side Span 6	1/2"	38'-9"	2	77'-6"		
SW67	" " " "	1/2"	38'-10"	2	77'-8"		
SW68	" " " "	1/2"	38'-9"	2	77'-6"		
SC1	Curbs Spans 1, 2, 4, 5 & 6	1/2"	4'-3"	658	2796'-6"		
HAND RAIL							
R1	End Posts	1/2"	6'-6"	24	156'-0"		
R2	" " "	1/2"	6'-2"	8	49'-4"		
R31	Int Posts Span 1	1/2"	2'-6"	18	45'-0"		Low Side
R31	" " " "	1/2"	2'-6"	30	75'-0"		Both Sides
R31	" " " "	1/2"	2'-6"	12	30'-0"		Low Side
R32	" " " "	1/2"	2'-9"	18	49'-6"		High Side
R32	" " " "	1/2"	2'-9"	12	33'-0"		" " "
R32	" " " "	1/2"	2'-9"	12	33'-0"		" " "
R33	" " " "	1/2"	3'-2"	12	38'-0"		" " "
R33	" " " "	1/2"	3'-2"	24	76'-0"		Both Sides
R34	" " " "	1/2"	3'-8"	12	44'-0"		Low Side
R34	" " " "	1/2"	3'-8"	12	44'-0"		" " "
R41	" " " "	1/2"	2'-0"	6	12'-0"		Both Sides
R41	" " " "	1/2"	2'-0"	10	20'-0"		Low Side
R41	" " " "	1/2"	2'-0"	4	8'-0"		High Side
R42	" " " "	1/2"	2'-3"	6	13'-6"		" " "
R42	" " " "	1/2"	2'-3"	4	9'-0"		" " "
R42	" " " "	1/2"	2'-3"	4	9'-0"		" " "
R43	" " " "	1/2"	2'-8"	4	10'-8"		" " "
R43	" " " "	1/2"	2'-8"	8	21'-4"		Both Sides
R44	" " " "	1/2"	3'-2"	4	12'-8"		Low Side
R44	" " " "	1/2"	3'-2"	4	12'-8"		" " "
R44	Int Posts Span 5	1/2"	3'-2"	4	12'-8"		" " "
R51	Pier Posts Span 1	1/2"	1'-6"	12	18'-0"		Both Sides
R51	" " " "	1/2"	1'-6"	12	18'-0"		High Side
R51	" " " "	1/2"	1'-6"	24	36'-0"		Both Sides
R51	" " " "	1/2"	1'-6"	12	18'-0"		High Side
R51	" " " "	1/2"	1'-6"	12	18'-0"		" " "
R52	" " " "	1/2"	1'-10"	12	22'-0"		Low Side
R52	" " " "	1/2"	1'-10"	12	22'-0"		" " "
R52	" " " "	1/2"	1'-10"	24	44'-0"		Both Sides
R53	" " " "	1/2"	2'-2"	12	26'-0"		Low Side
R61	" " " "	1/2"	1'-0"	4	4'-0"		Both Sides
R61	" " " "	1/2"	1'-0"	4	4'-0"		High Side
R61	" " " "	1/2"	1'-0"	8	8'-0"		Both Sides
R61	" " " "	1/2"	1'-0"	4	4'-0"		High Side
R61	" " " "	1/2"	1'-0"	4	4'-0"		" " "
R62	" " " "	1/2"	1'-4"	4	5'-4"		Low Side
R62	" " " "	1/2"	1'-4"	4	5'-4"		" " "
R62	" " " "	1/2"	1'-4"	4	5'-4"		Both Sides
R63	Pier Posts Span 2	1/2"	1'-8"	4	6'-8"		Low Side
R71	Railing Span 1	3/4"	5'-9"	48	276'-0"		High Side
R71	" " " "	3/4"	5'-9"	24	138'-0"		" " "
R72	" " " "	3/4"	6'-9"	24	162'-0"		Low Side
R72	" " " "	3/4"	6'-9"	24	162'-0"		High Side
R72	" " " "	3/4"	6'-9"	18	121'-6"		" " "
R73	" " " "	3/4"	7'-9"	24	186'-0"		Low Side
R73	" " " "	3/4"	7'-9"	48	372'-0"		" " "
R73	" " " "	3/4"	7'-9"	18	139'-6"		High Side
R73	" " " "	3/4"	7'-9"	18	139'-6"		Low Side
R74	" " " "	3/4"	8'-9"	36	315'-0"		" " "
R74	" " " "	3/4"	8'-9"	144	1260'-0"		Both Sides
R74	" " " "	3/4"	8'-9"	18	157'-6"		Low Side
R75	Railing Span 6	3/4"	9'-9"	36	351'-0"		High Side
P1	Railing Vertical	1/2"	3'-6"	1654	5789'-0"		
SC2	Curbs Span No 3	1/2"	5'-5"	110	595'-10"		
Br1	All Col. Brackets	1/2"	6'-0"	240	1440'-0"		
Br2	Int. " " "	1/2"	5'-3"	100	525'-0"		
Br3	All " " "	1/2"	4'-9"	240	1140'-0"		
Br4	" " " "	1/2"	4'-0"	120	480'-0"		
Br5	Out " " "	1/2"	3'-8"	20	73'-4"		
SC3	Sidewalk, Span No 3	1/2"	2'-2"	110	236'-4"		

MARK	LOCATION	SIZE	LGTH.	No BARS	TOTAL LENGTH	SKETCH	O.to.O.
CB1	Catch Basin End	1/2"	6'-7"	4	26'-4"		
CB3	Catch Basin End	1/2"	8'-9"	4	35'-0"		
CB2	Catch Basins	1/2"	5'-3"	8	42'-0"		
DC-1	Dividing Curb	1/2"	2'-3"	746	1678'-6"		1'-8 1/2"
DC-2	" " "	1/2"	10'-6"	40	420'-0"		
DC-3	" " "	1/2"	2'-4"	40	85'-4"		
DC-4	" " "	1/2"	9'-0"	8	72'-0"		
DC-5	Dividing Curb	1/2"	18'-4"	8	146'-8"		

8,998.00 Lin. Ft. of 1/4" @ 5.313#/ft = 47,806.0  
 75,304.00 " " of 1/8" @ 4.303#/ft = 324,033.0  
 14,822.33 " " of 1" @ 2.670#/ft = 39,576.0  
 23,980.00 " " of 3/4" @ 1.502#/ft = 36,018.0  
 169,843.83 " " of 1/2" @ 0.850#/ft = 144,367.0  
 111,840.50 " " of 1/2" @ 0.668#/ft = 74,709.0  
 16,424.00 " " of 1/4" @ 0.167#/ft = 2,743.0

Total = 669,252.0-Lbs.

Total includes bars in Precast Beams

\* Note: Bars in Precast Beams to be omitted if Alternate is used.

MARK	LOCATION	SIZE	LGTH.	No BARS	TOTAL LENGTH	SKETCH	O.to.O.
B39	Int Steel Beams	3/8"	28'-4"	180	5100'-0"		
B40	" " "	1/2"	0'-8"	1020	340'-0"		
B41	Int Steel Beams	1/2"	10'-4"	510	5270'-0"		
B42	Outside Steel Beams	1/2"	11'-2"	202	2255'-8"		
B43	" " "	1/2"	1'-0"	404	404'-0"		
B44	" " "	3/8"	28'-10"	36	1036'-0"		
D10	Web Stiff Int	1"	11'-6"	18	207'-0"		9'-4"
D11	" " Sidewalk	1"	7'-6"	2	15'-0"		5'-4"
D12	" " "	1"	8'-6"	2	17'-0"		6'-4"
D51	Web Stiffeners	1/2"	7'-9"	63	489'-3"		

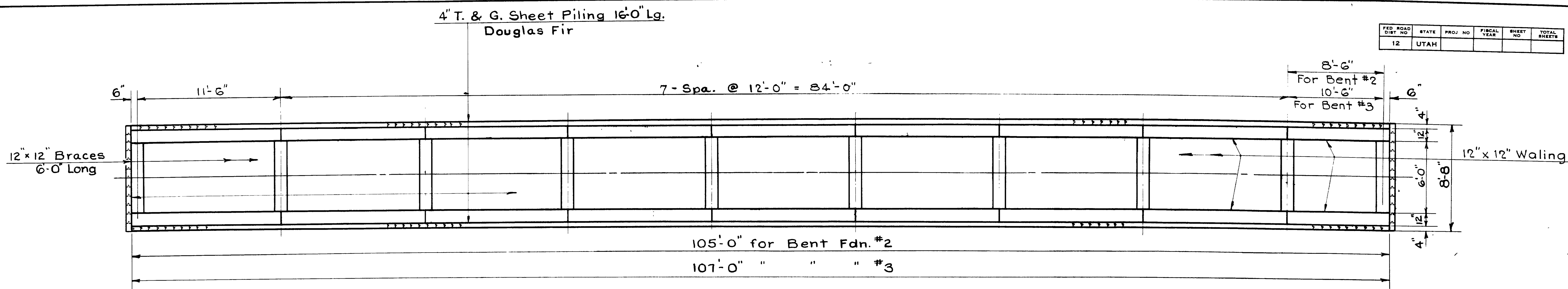
8,998.00 Lin. Ft. of 1/4" @ 5.313#/ft = 47,806.0  
 75,304.00 " " of 1/8" @ 4.303#/ft = 324,033.0  
 14,822.33 " " of 1" @ 2.670#/ft = 39,576.0  
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Total = 669,252.0-Lbs.

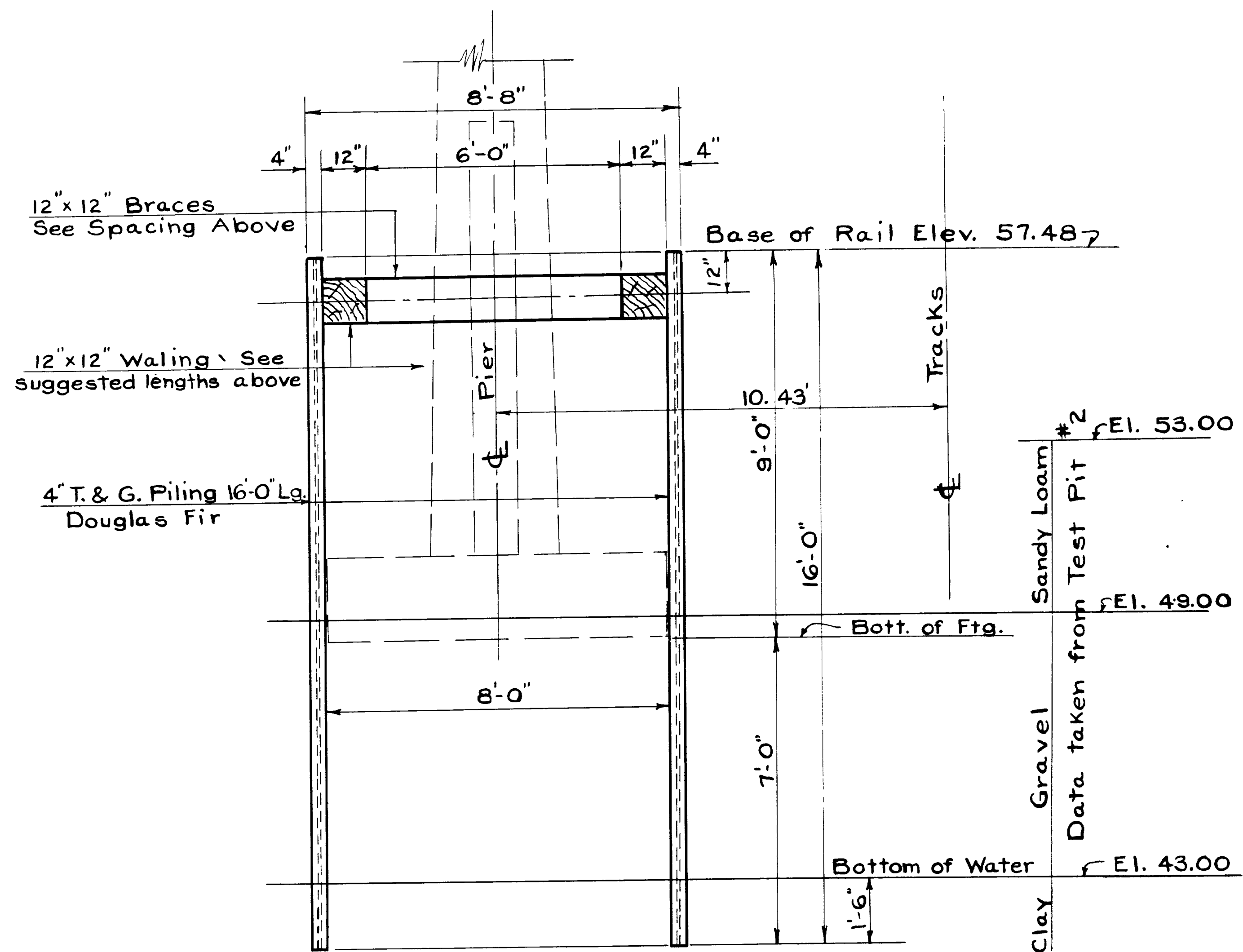
Total does not include bars in Precast Beams

REVISIONS	DATE	BY	NO.

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
12	UTAH				



PLAN OF TRACK BULKHEAD  
1/4" = 1'-0"



TYPICAL SECTION OF BULKHEAD  
1/2" = 1'-0"

Note: Payment for furnishing, placing and removal of bulkheads shall be included in unit bid price paid for excavation.

REVISIONS	DATE	BY	REASON

Sheet "A"

UTAH STATE ROAD COMMISSION  
SALT LAKE CITY, UTAH  
BRIDGE DEPARTMENT

RIVERDALE OVERHEAD  
OVER U.P.R.R. TRACKS  
Sta. 121+14.47 U.G.I. 31(5)  
Riverdale Revision, Weber Co.

DESIGNED BY: F.M.E.	SCALE: As Noted
DRAWN BY: R.W.L.	ISSUED: 8/21/45
CHECKED BY:	APPROVED: <i>C. V. Peterson</i> CHIEF BRIDGE ENGINEER
EXAMINED BY:	BR NO. 29-21-4-7 DRG NO. D-485

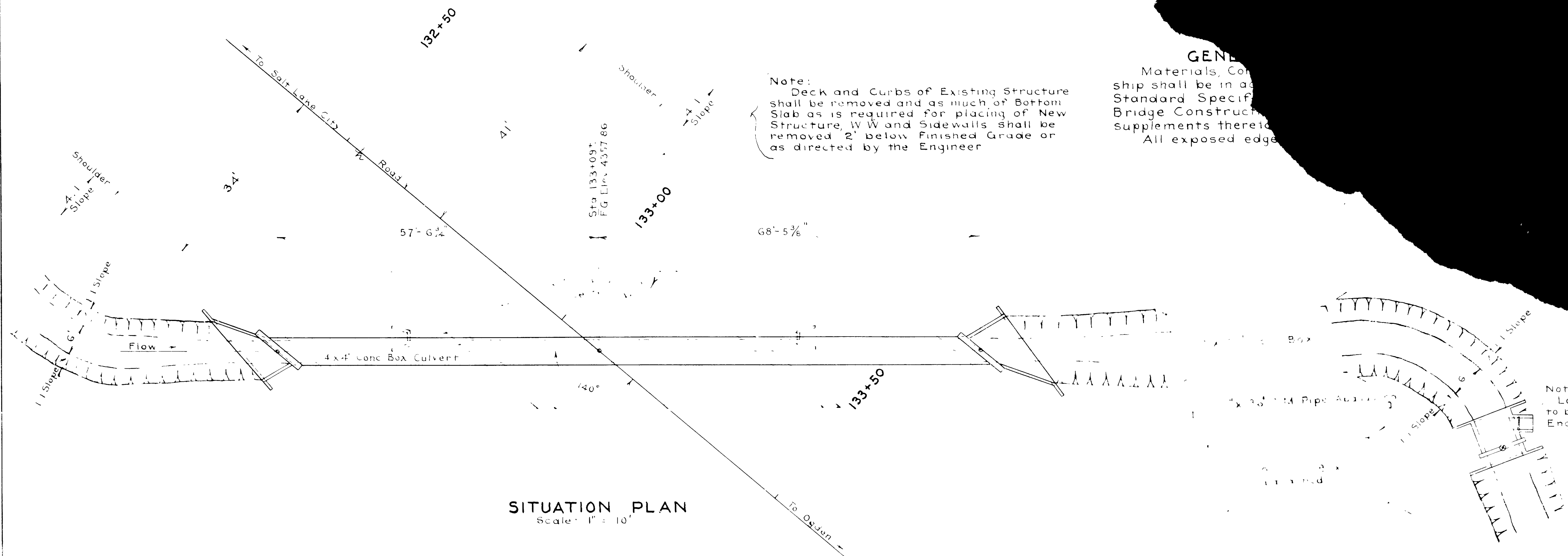
Bridge Engineer - Professional Engineer - License 366  
APPROVED

STRUCTURAL ENGINEER  
Prof. Engr. License

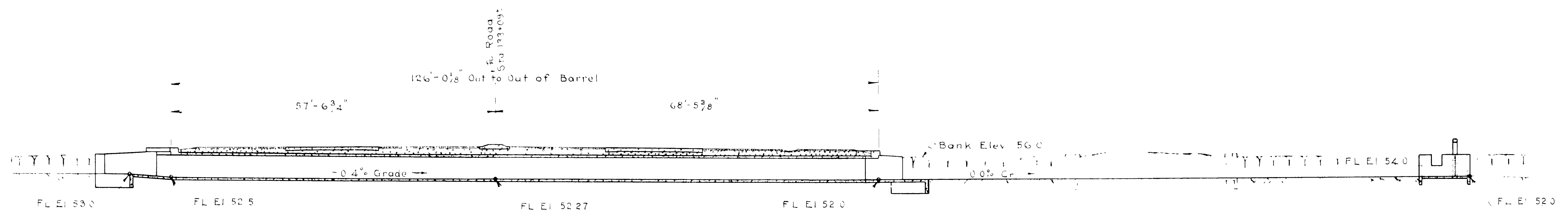


Note:  
Deck and Curbs of Existing Structure shall be removed and as much of Bottom Slab as is required for placing of New Structure, W.W. and Sidewalls shall be removed 2' below Finished Grade or as directed by the Engineer

GENERAL  
Materials, Construction shall be in accordance with Standard Specifications for Bridge Construction and supplements thereto. All exposed edges shall be finished.



SITUATION PLAN  
Scale: 1" = 10'



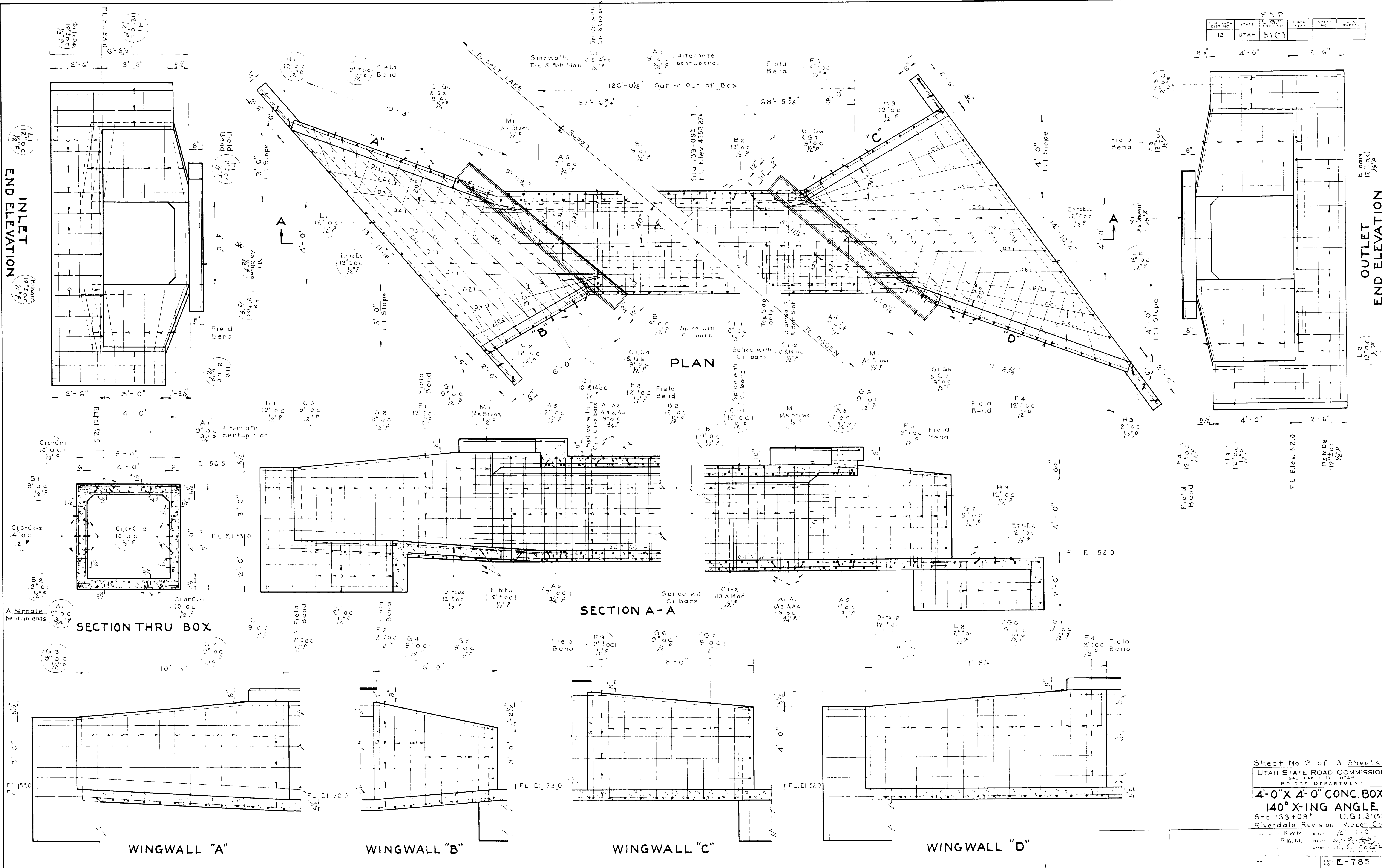
PROFILE ALONG C/S OF BOX & CANAL  
Scale: 1" = 10'

**QUANTITIES**

Excavation for Structure	104 C.Y.
Channel Excavation	400 C.Y.
Concrete (Class "A")	59 C.Y.
Reinforcing Steel	10,877 Lbs.
Removal of Existing Structure	1 Each

Sheet No. 1 of 3 Sheets  
UTAH STATE ROAD COMMISSION  
SALT LAKE  
BRIDGE DEPARTMENT  
**4'-0" X 4'-0" CONC. BOX**  
**140° X-ING ANGLE**  
Sta. 133+00 UGI 310  
Regional Division, Weber Co.  
E-785

F.A.P.					
FED. ROAD DIST. NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
12	UTAH	31 (F)			



NO.	DATE	BY	REVISIONS

Sheet No. 2 of 3 Sheets  
 UTAH STATE ROAD COMMISSION  
 SALT LAKE CITY, UTAH  
 BRIDGE DEPARTMENT  
**4'-0" X 4'-0" CONC. BOX**  
**140° X-ING ANGLE**  
 Sta 133+09' U.G.I.31(s)  
 Riverdale Revision Weber Co  
 R.W.M. 1/2" x 1'-0"  
 6/2/59  
 E-785



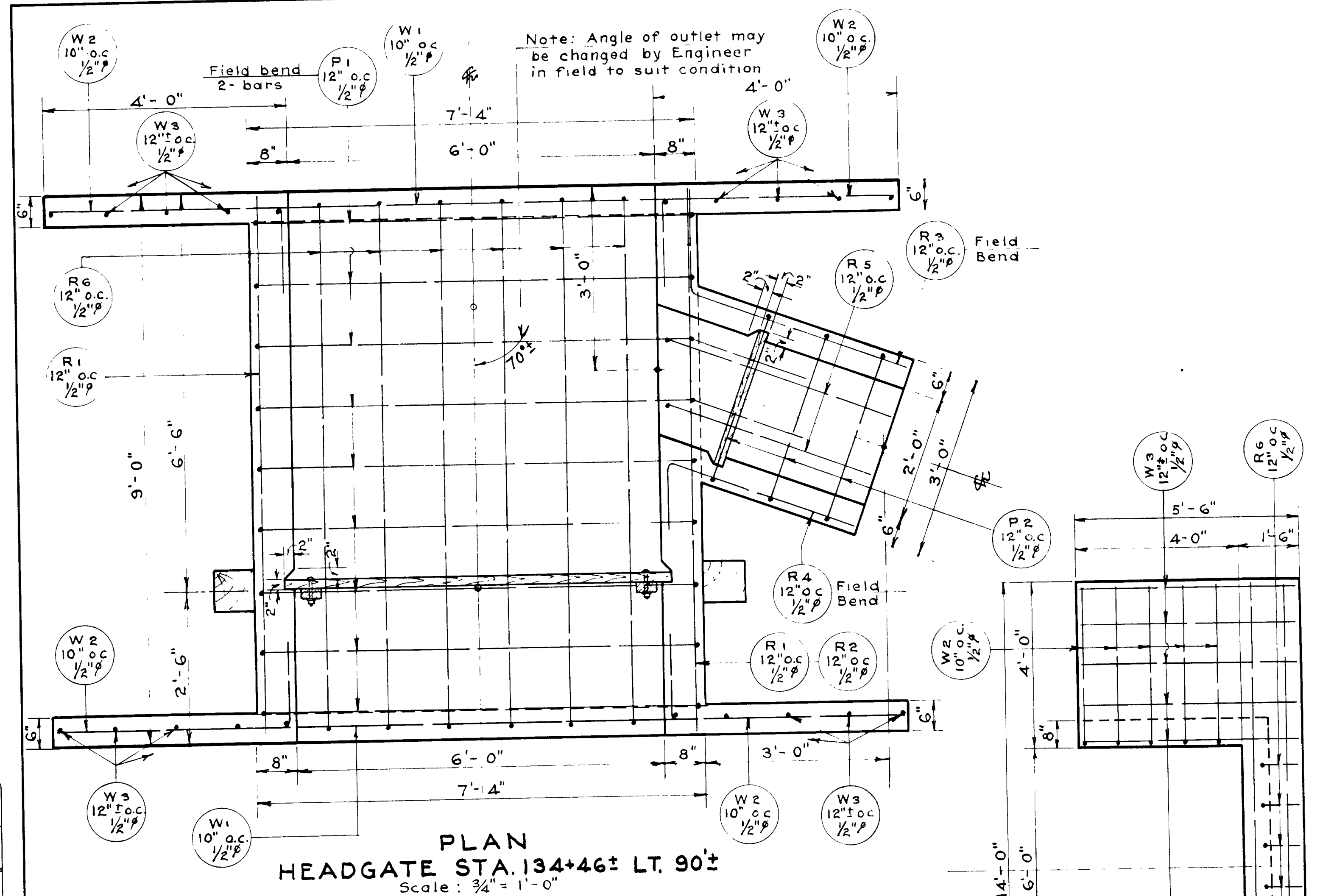
### REINF. STEEL SCHEDULE

MARK	LOCATION	SIZE	L'GTH	No BARS	TOTAL LENGTH	SKETCH	O.to.O.
A1	Top & Bott Slab	3/4"	6'-6"	320	2080'-0"		4'-9"
A2	Slab Ends T&B	3/4"	5'-6"	4	22'-0"	"a" = 2'-7"	3'-9"
A3	"	3/4"	4'-11"	4	19'-8"	"a" = 2'-0"	3'-2"
A4	"	3/4"	4'-3"	4	17'-0"	9/32" Hooks	2'-6"
A5	"	3/4"	9'-2"	8	73'-4"	9/32" Hooks	7'-6"
B1	Sidewalls	1/2"	9'-4"	330	3080'-0"		
B2	"	1/2"	5'-4"	252	1344'-0"		
C1	Slabs & Sidewalls		40'-0"	114	4560'-0"		
C1-1	Top Slab		12'-0"	9	108'-0"		
C1-2	Sidewalls & B. Slab		14'-0"	29	406'-0"		
D1	Inlet Slab		10'-9"	2	21'-0"	"a" = 8'-6"	
D2	"		10'-1"	3	30'-3"	"a" = 7'-10"	
D3	"		9'-6"	3	28'-6"	"a" = 7'-3"	
D4	"		8'-11"	2	17'-10"	"a" = 6'-8"	
D5	Outlet Slab		12'-9"	2	25'-6"	"a" = 10'-6"	
D6	"		12'-4"	3	37'-0"	"a" = 10'-1"	
D7	"		11'-10"	3	35'-6"	"a" = 9'-7"	
D8	"		11'-5"	2	22'-10"	"a" = 9'-2"	
E1	Inlet Slab		9'-1"	1	9'-1"		
E2	"		10'-1"	1	10'-1"		
E3	"		11'-0"	1	11'-0"		
E4	"		11'-9"	1	11'-9"		
E5	"		12'-7"	1	12'-7"		
E6	"		13'-6"	1	13'-6"		
E7	Outlet Slab		8'-10"	1	8'-10"		
E8	"		9'-5"	1	9'-5"		
E9	"		10'-0"	1	10'-0"		
E10	"		10'-8"	1	10'-8"		
E11	"		11'-7"	1	11'-7"		
E12	"		12'-6"	1	12'-6"		
E13	"		13'-4"	1	13'-4"		
E14	"		14'-4"	1	14'-4"		
F1	Wingwall "A"		13'-10"	6	83'-0"		Field Bend
F2	" "B"		9'-2"	6	55'-0"		"
F3	" "C"		11'-2"	6	67'-0"		"
F4	" "D"		15'-0"	6	90'-0"		"
G1	WW "A", "B", "C" & "D"		7'-0"	8	56'-0"	"a" = 5'-0"	
G2	Wingwall "A"		6'-5"	5	32'-1"	"a" = 4'-5"	
G3	"		5'-11"	5	29'-7"	"a" = 3'-11"	
G4	" "B"		6'-3"	3	18'-9"	"a" = 4'-3"	
G5	"		5'-5"	4	21'-8"	"a" = 3'-5"	
G6	" "C" & "D"		6'-8"	10	66'-8"	"a" = 4'-8"	
G7	"		6'-4"	11	69'-8"	"a" = 4'-4"	
H1	Inlet Cutoff Wall		5'-9"	3	17'-3"		
H2	"		5'-3"	3	15'-9"		
H3	Outlet "		6'-3"	6	37'-6"		
L1	Inlet Cutoff Wall		18'-9"	3	56'-3"		
L2	Outlet "		19'-8"	3	59'-0"		
M1	Curb		9'-9"	4	39'-0"		
P1	Headgate		15'-11"	9	143'-3"	"a" = 7'-1"	Field Bend
P2	Headgate Outlet		7'-3"	3	21'-9"	"a" = 2'-9"	
R1	Headgate		8'-9"	8	70'-0"		
R2	"		4'-5"	2	8'-10"		
R3	" Outlet		5'-6"	4	22'-0"		Field Bend
R4	"		4'-3"	4	17'-0"		"
R5	"		6'-0"	2	12'-0"		"

### REINF. STEEL SCHEDULE

MARK	LOCATION	SIZE	L'GTH	No BARS	TOTAL LENGTH	SKETCH	O.to.O.
R6	Headgate Slab	1/2"	10'-3"	6	61'-6"		
W1	Headgate W.W.		13'-9"	4	55'-0"		
W2	"		5'-2"	20	103'-4"		
W3	"		5'-3"	20	105'-0"		

11,309'-5" of 1/2" @ 0.668# = 7554.69#  
 2,212'-0" of 3/4" @ 1.502# = 3322.42#  
**TOTAL = 10,877.11#**



### END ELEVATION SHOWING WINGWALLS

Scale: 1/2" = 1'-0"

